

Madison, Wisconsin

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CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

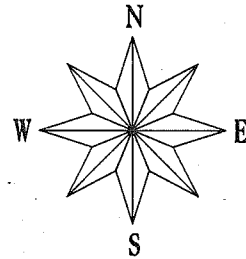
PLAN OF PROPOSED IMPROVEMENT

EIGHTH ADDITION TO BLACKHAWK SUBDIVISION: PHASE 9

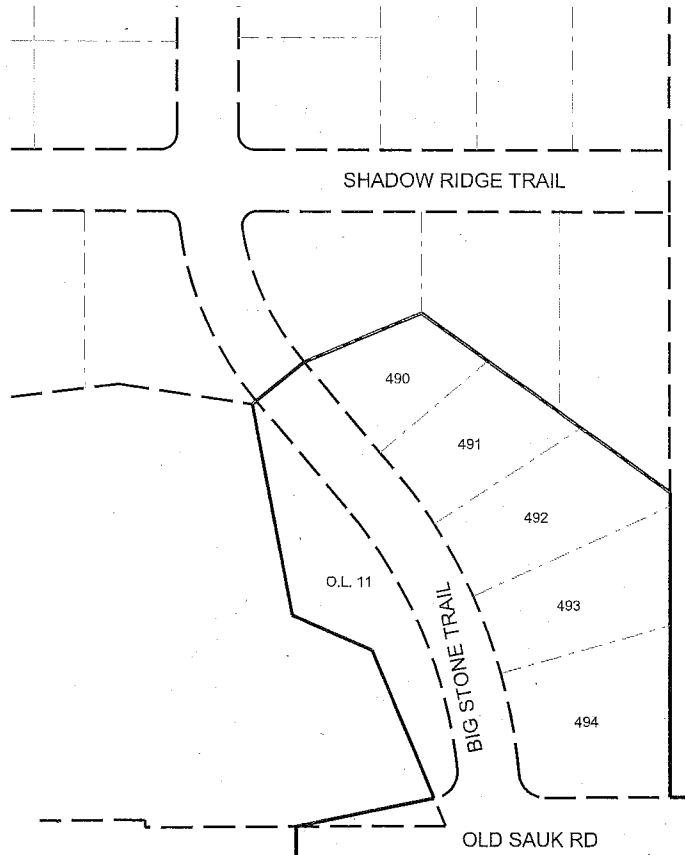
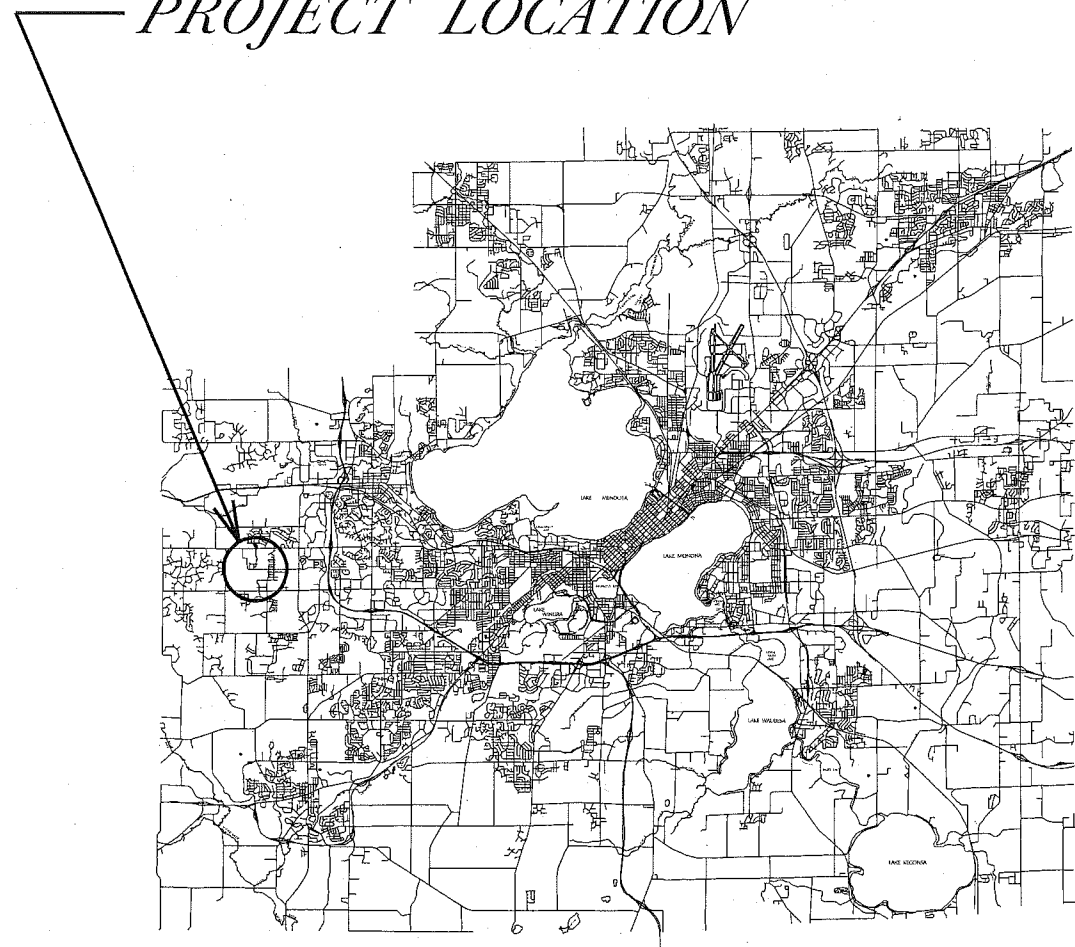
CITY PROJECT NO. 11259

CITY CONTRACT NO. 7682

MUNIS NO. 11259



PROJECT LOCATION



PUBLIC IMPROVEMENT PROJECT APPROVED

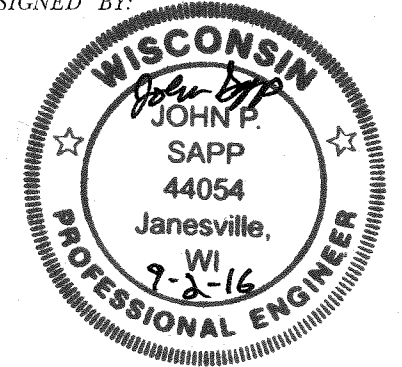
JUNE 21, 2016

BY THE COMMON COUNCIL OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN APPROVED BY:

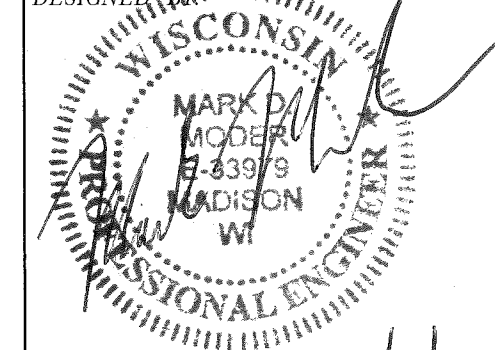
[Signature] 9/6/16
 City Engineer Date

STREET DESIGNED BY:



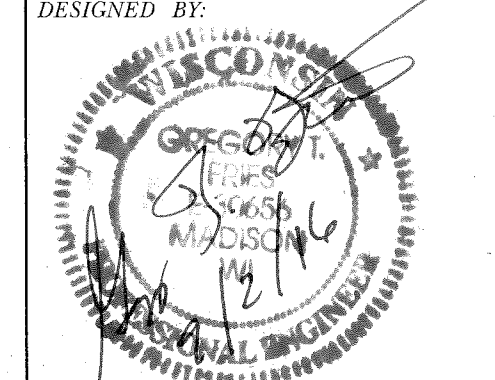
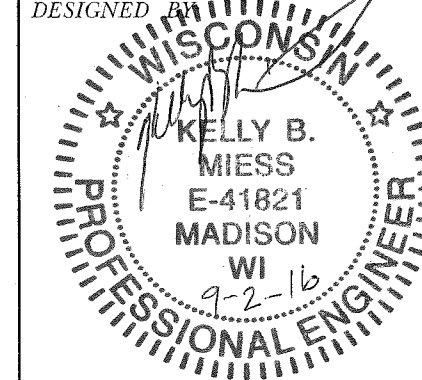
ELECTRICAL DESIGNED BY:

SANITARY SEWER DESIGNED BY:



WATER DESIGNED BY:

STORM SEWER DESIGNED BY:



PLOT SCALE:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

OLD SAUK ROAD SHALL BE TYPE C PAVEMENT PER STANDARD DETAIL DRAWING 4.02.

BIG STONE TRAIL SHALL BE TYPE B PAVEMENT PER STANDARD DETAIL DRAWING 4.02.

UNDERDRAINS SHALL BE INSTALLED, PER STANDARD DETAIL DRAWING 4.05 FOR 75' ON EACH SIDE OF THE LOW POINT, OR TO THE NEAREST CURB HIGH POINT. ALL UNDERDRAIN SHALL BE WRAPPED.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADES OF 0.5% TOWARD STORM SEWER INLETS.

ALL DITCHES SHALL DRAIN WITH A MINIMUM GRADES OF 0.5%

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL BE 1.5%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.03. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4.00:1. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENT FROM THE CITY ENGINEER PRIOR TO STAKING THIS PROJECT.

CURB STATION AND OFFSETS SHALL BE TO THE FACE OF CURB UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE TOP OF CURB (OR EXTENDED TOP OF CURB FOR DRIVEWAYS OR RAMPS) UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO SDD 3.05.

THE DEVELOPER/CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO CITY OF MADISON TRAFFIC ENGINEERING DIVISION A MINIMUM OF 10 WORKING DAYS PRIOR TO THE ANTICIPATED START OF WORK.

TRAFFIC CONTROL ROAD CLOSURE SIGNS OR BARRICADES ON THE SOUTH END OF BIG STONE TRAIL SHALL REMAIN IN PLACE UNTIL PROPER SIGHT DISTANCE HAS BEEN ACHIEVED. PERMANENT ROW MUST BE ACQUIRED FOR SIGHT DISTANCE, AS SHOWN ON SHEET P-3. CONTACT CITY TRAFFIC ENGINEERING TO CONFIRM THAT SIGHT DISTANCE HAS BEEN ACHIEVED PRIOR TO REMOVING TRAFFIC CONTROL ROAD CLOSURE SIGNS OR BARRICADES..

WORK WITHIN OLD SAUK ROAD SHALL ONLY TAKE PLACE ON WEEKDAYS BETWEEN 8:30 A.M. AND 4:00 P.M. AND NORMAL WORKING HOURS ON WEEKENDS. DURING THE PEAK TRAFFIC HOURS OF 7:00 A.M. TO 8:30 A.M. AND 4:00 P.M. TO 5:30 P.M. BOTH LANES OF TRAFFIC SHALL BE OPEN. DURING OFF PEAK WORK HOURS, TRAFFIC MAY BE REDUCED TO ONE LANE AND FLAGGERS SHALL BE USED TO MAINTAIN TRAFFIC IN BOTH DIRECTIONS. THE WORK AREA SHALL BE PATCHED WITH ASPHALT OR PLATED WHEN REOPENED TO TRAFFIC.

THE LOCATION AND INFORMATION FOR PROPOSED NEW TREES, IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS ARE APPROXIMATE AND ARE SHOWN FOR REFERENCE ONLY. THE LOCATIONS, SPECIFICATIONS AND PLANTING METHODS OF ALL PROPOSED NEW OR REPLACEMENT TREES IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE APPROVED BY THE CITY FORESTER PRIOR TO INSTALLATION.

NO TREES IN THE RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE TRIMMED, PRUNED, REMOVED OR ADVERSELY AFFECTED IN ANY WAY UNTIL THE DEVELOPER HAS RECEIVED WRITTEN PERMISSION FROM THE CITY ENGINEER OR CITY FORESTER. SAID WRITTEN PERMISSION SHALL INCLUDE LANGUAGE INDICATING THAT SECTION 10.101 OF THE MADISON GENERAL ORDINANCES AND ADMINISTRATIVE PROCEDURE MEMORANDUM NO. 6-2, REFERRING TO NOTIFICATION OF PROPERTY OCCUPANTS AND/OR OWNERS, HAS BEEN COMPLIED WITH.

CONVENTIONAL SIGNS

FIELD VERIFY ALL UTILITY LOCATIONS

GAS ——— G ———

STORM SEWER ——— ST ———

SANITARY SEWER ——— SAN ———

WATER ——— W ———

OVERHEAD ELECTRIC ——— OH ———

POWER POLE 

ADA COMPLIANT RAMP W/
DETECTABLE WARNING FIELD 

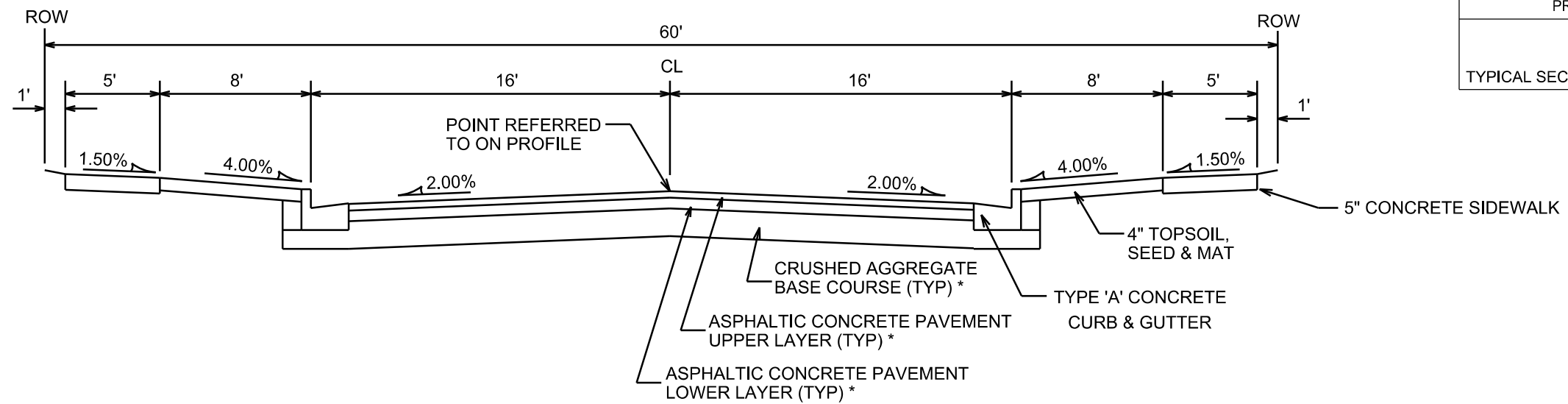
COMBUSTIBLE FLUIDS 

PLOT SCALE: _____

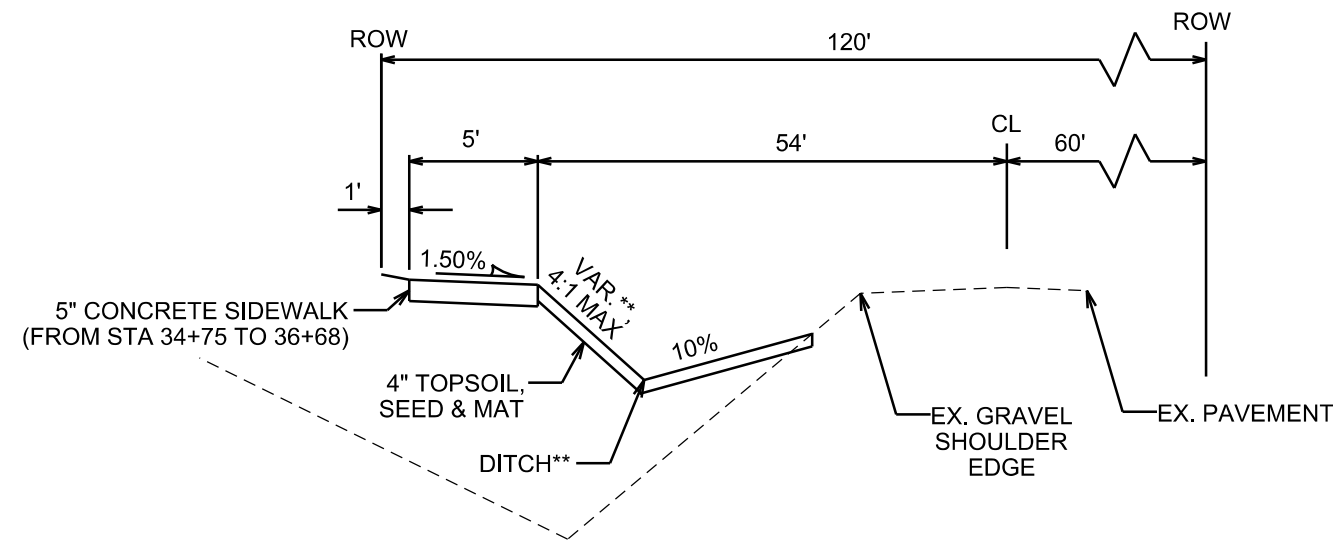
PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



TYPICAL SECTION
BIG STONE TRAIL



TYPICAL SECTION
OLD SAUK ROAD: STAGE 1
STA 32+47 TO STA 37+06

CITY OF MADISON MINIMUM PAVEMENT DESIGN

TYPE	CRUSHED AGG. BASE COURSE		ASPHALTIC CONCRETE PAVEMENT			
	LOWER LAYER GRADATION 1	UPPER LAYER GRADATION 2	LOWER LAYER TYPE	LOWER LAYER THICKNESS	UPPER LAYER TYPE	UPPER LAYER THICKNESS
A	6"	4"	E-0.3	1.75"	E-0.3	1.75"
B	6"	4"	E-1	2.25"	E-1	2"
C	6"	4"	E-3	3.25"	E-3	2"

NOTES:

TYPICAL SECTIONS NOT TO SCALE

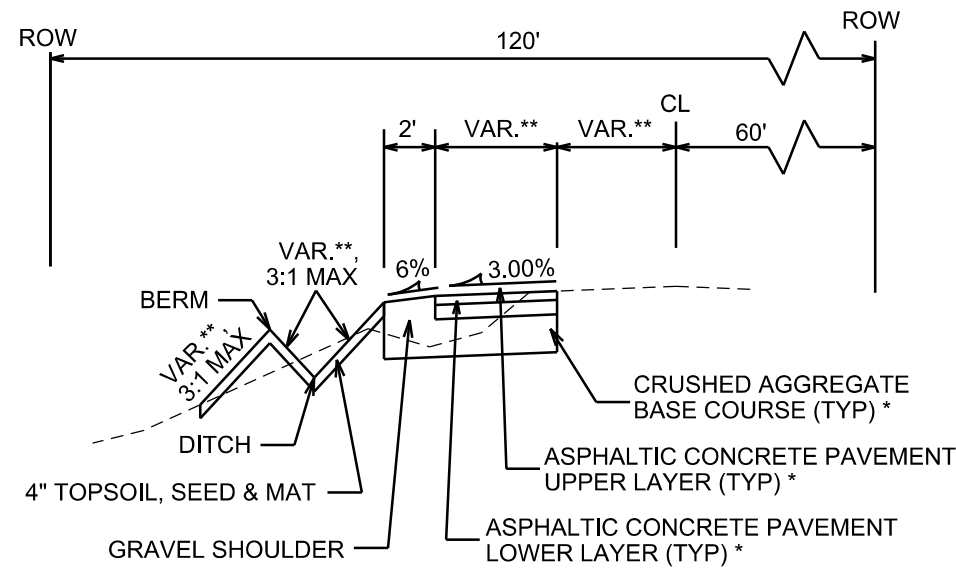
* BIG STONE TRAIL TO BE CONSTRUCTED AS TYPE 'B' PAVEMENT PER CITY OF MADISON MINIMUM PAVEMENT DESIGN

PLOT SCALE: _____

PLOT NAME: _____

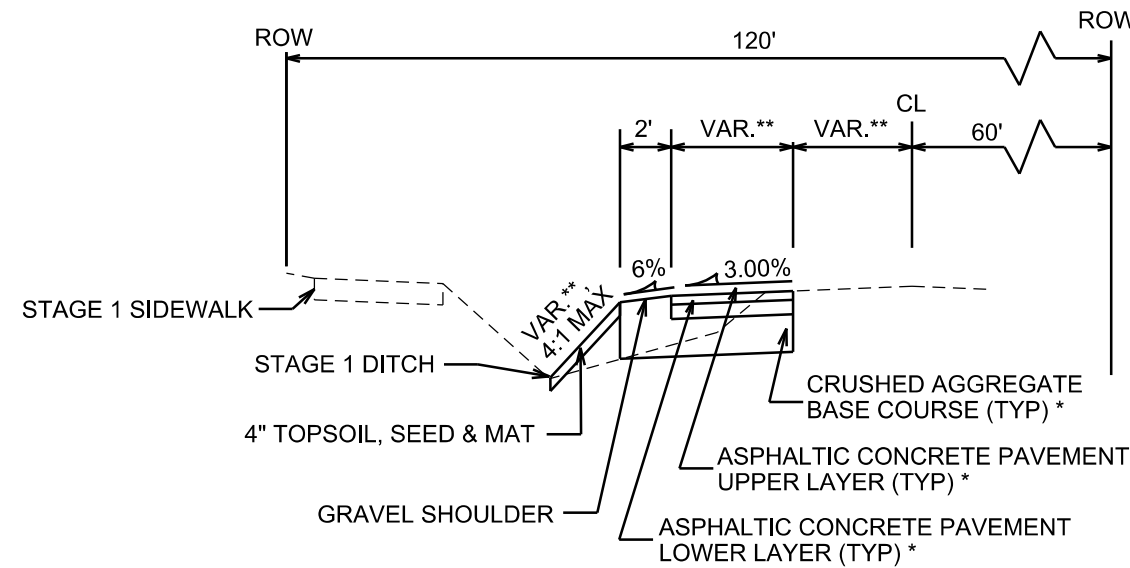
REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



TYPICAL SECTION

OLD SAUK ROAD
STA 33+10 TO STA 34+75



TYPICAL SECTION

OLD SAUK ROAD
STA 34+75 TO STA 36+82

CITY OF MADISON MINIMUM PAVEMENT DESIGN

TYPE	CRUSHED AGG. BASE COURSE		ASPHALTIC CONCRETE PAVEMENT			
	LOWER LAYER GRADATION 1	UPPER LAYER GRADATION 2	LOWER LAYER TYPE	LOWER LAYER THICKNESS	UPPER LAYER TYPE	UPPER LAYER THICKNESS
A	6"	4"	E-0.3	1.75"	E-0.3	1.75"
B	6"	4"	E-1	2.25"	E-1	2"
C	6"	4"	E-3	3.25"	E-3	2"

NOTES:

TYPICAL SECTIONS NOT TO SCALE

* OLD SAUK ROAD TO BE CONSTRUCTED AS TYPE 'C' PAVEMENT PER CITY OF MADISON MINIMUM PAVEMENT DESIGN.

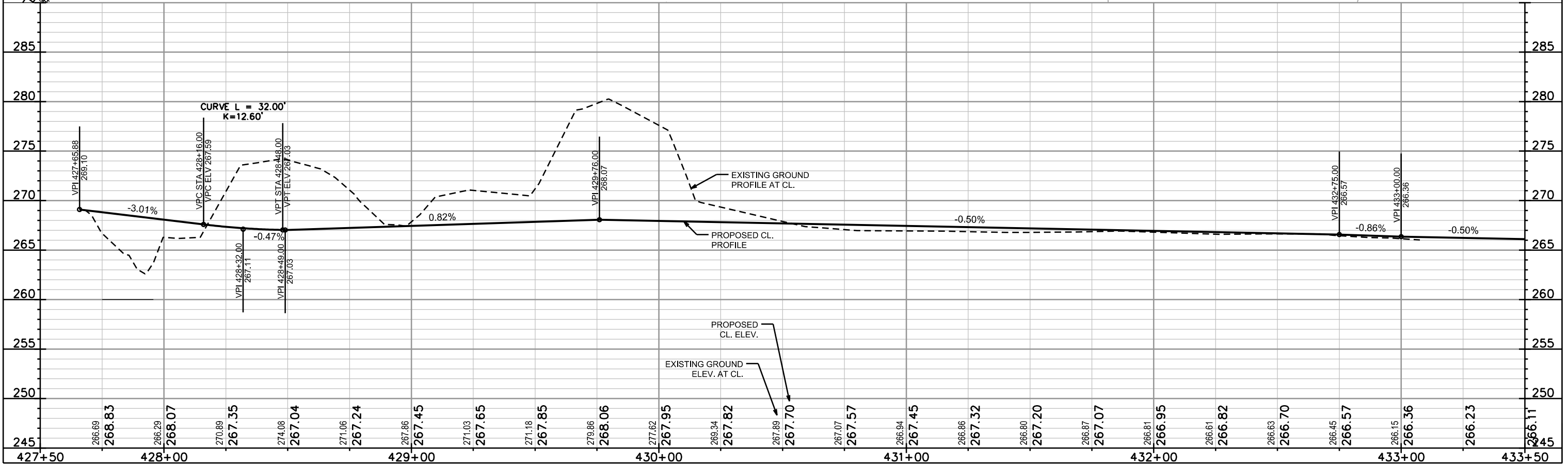
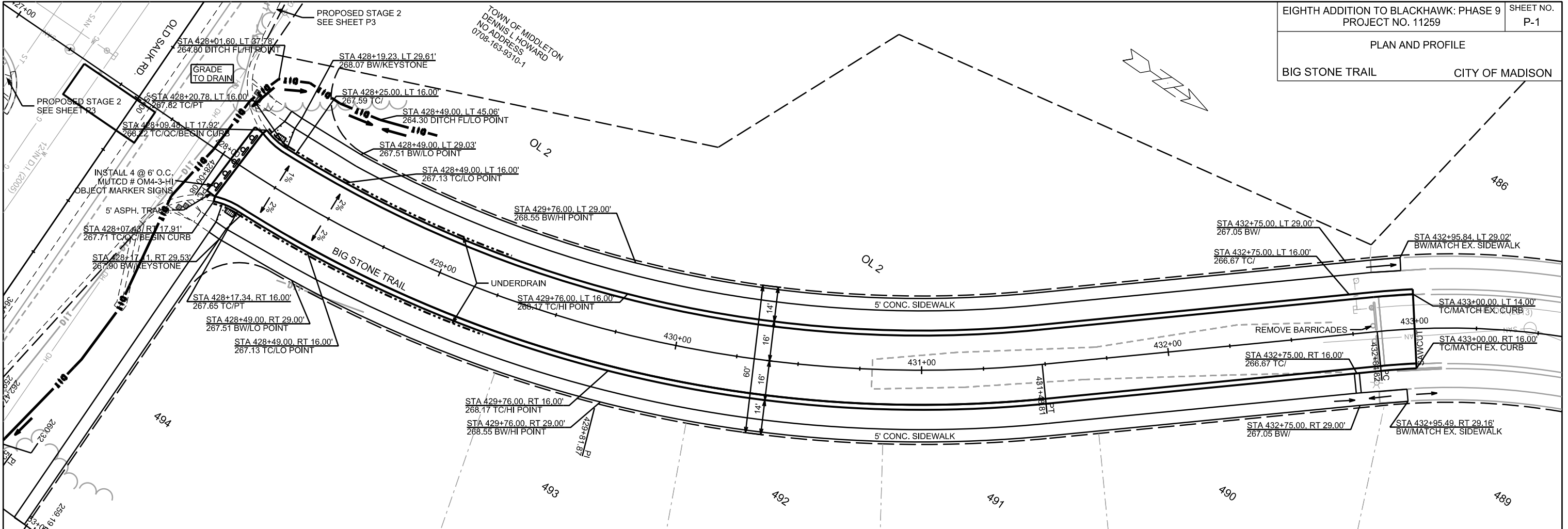
** SEE CROSS SECTIONS FOR SLOPES, OFFSETS, AND ELEVATIONS.

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

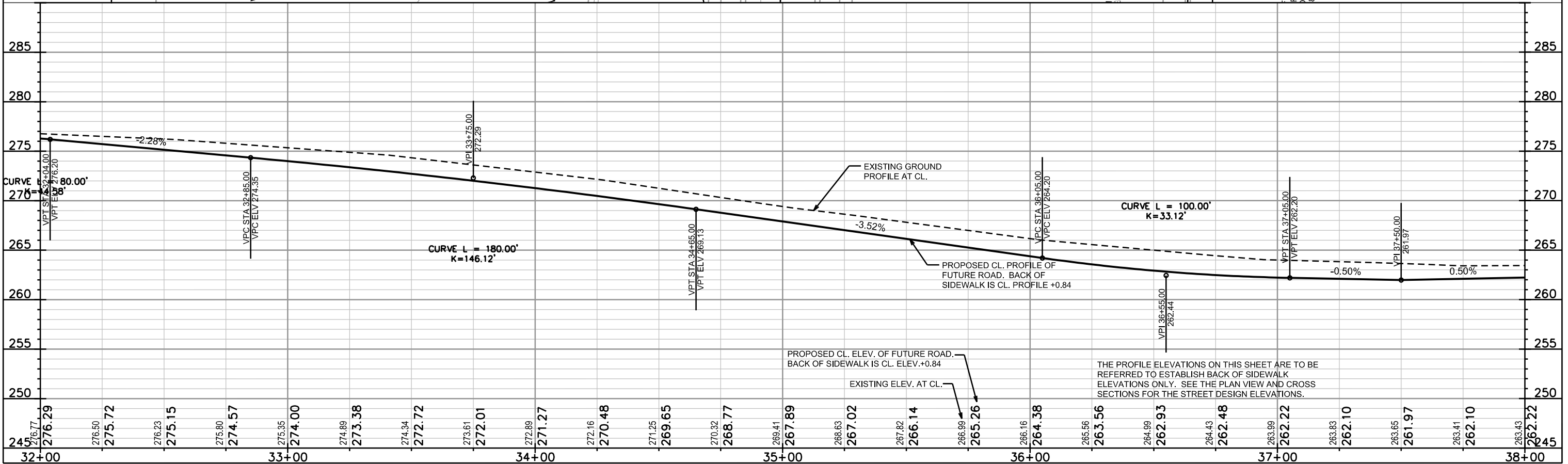
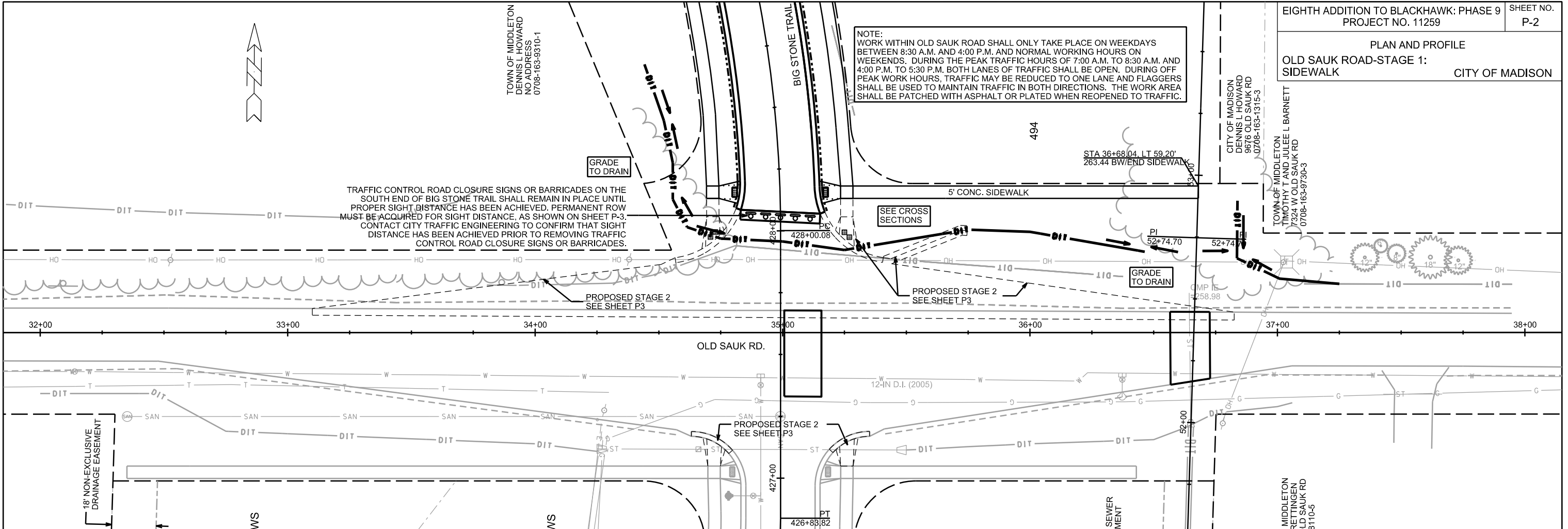


PLOT SCALE: _____
 PLOT NAME: _____
 REV. DATE: _____
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLAN AND PROFILE
OLD SAUK ROAD-STAGE 1:
SIDEWALK CITY OF MADISON

NOTE:
WORK WITHIN OLD SAUK ROAD SHALL ONLY TAKE PLACE ON WEEKDAYS BETWEEN 8:30 A.M. AND 4:00 P.M. AND NORMAL WORKING HOURS ON WEEKENDS. DURING THE PEAK TRAFFIC HOURS OF 7:00 A.M. TO 8:30 A.M. AND 4:00 P.M. TO 5:30 P.M. BOTH LANES OF TRAFFIC SHALL BE OPEN. DURING OFF PEAK WORK HOURS, TRAFFIC MAY BE REDUCED TO ONE LANE AND FLAGGERS SHALL BE USED TO MAINTAIN TRAFFIC IN BOTH DIRECTIONS. THE WORK AREA SHALL BE PATCHED WITH ASPHALT OR PLATED WHEN REOPENED TO TRAFFIC.

TRAFFIC CONTROL ROAD CLOSURE SIGNS OR BARRICADES ON THE SOUTH END OF BIG STONE TRAIL SHALL REMAIN IN PLACE UNTIL PROPER SIGHT DISTANCE HAS BEEN ACHIEVED. PERMANENT ROW MUST BE ACQUIRED FOR SIGHT DISTANCE, AS SHOWN ON SHEET P-3. CONTACT CITY TRAFFIC ENGINEERING TO CONFIRM THAT SIGHT DISTANCE HAS BEEN ACHIEVED PRIOR TO REMOVING TRAFFIC CONTROL ROAD CLOSURE SIGNS OR BARRICADES.



THE PROFILE ELEVATIONS ON THIS SHEET ARE TO BE REFERRED TO ESTABLISH BACK OF SIDEWALK ELEVATIONS ONLY. SEE THE PLAN VIEW AND CROSS SECTIONS FOR THE STREET DESIGN ELEVATIONS.

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

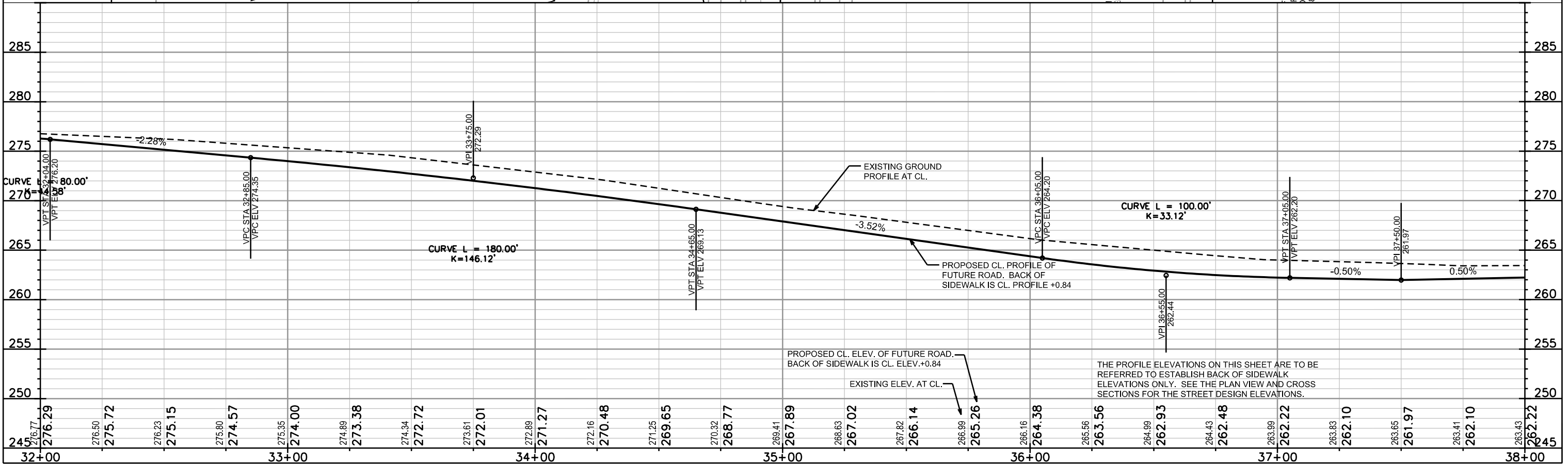
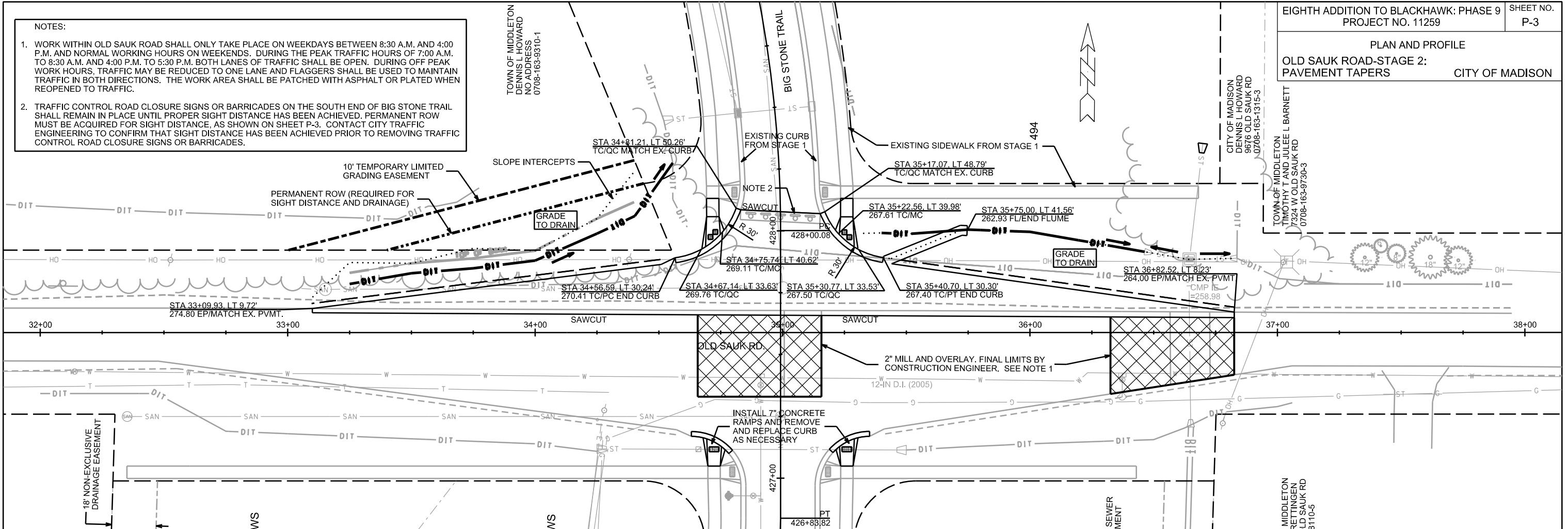
PLAN AND PROFILE
OLD SAUK ROAD-STAGE 2:
PAVEMENT TAPERS CITY OF MADISON

- NOTES:
- WORK WITHIN OLD SAUK ROAD SHALL ONLY TAKE PLACE ON WEEKDAYS BETWEEN 8:30 A.M. AND 4:00 P.M. AND NORMAL WORKING HOURS ON WEEKENDS. DURING THE PEAK TRAFFIC HOURS OF 7:00 A.M. TO 8:30 A.M. AND 4:00 P.M. TO 5:30 P.M. BOTH LANES OF TRAFFIC SHALL BE OPEN. DURING OFF PEAK WORK HOURS, TRAFFIC MAY BE REDUCED TO ONE LANE AND FLAGGERS SHALL BE USED TO MAINTAIN TRAFFIC IN BOTH DIRECTIONS. THE WORK AREA SHALL BE PATCHED WITH ASPHALT OR PLATED WHEN REOPENED TO TRAFFIC.
 - TRAFFIC CONTROL ROAD CLOSURE SIGNS OR BARRICADES ON THE SOUTH END OF BIG STONE TRAIL SHALL REMAIN IN PLACE UNTIL PROPER SIGHT DISTANCE HAS BEEN ACHIEVED. PERMANENT ROW MUST BE ACQUIRED FOR SIGHT DISTANCE, AS SHOWN ON SHEET P-3. CONTACT CITY TRAFFIC ENGINEERING TO CONFIRM THAT SIGHT DISTANCE HAS BEEN ACHIEVED PRIOR TO REMOVING TRAFFIC CONTROL ROAD CLOSURE SIGNS OR BARRICADES.

TOWN OF MIDDLETON
DENNIS L HOWARD
NO ADDRESS
0708-163-9310-1

CITY OF MADISON
DENNIS L HOWARD
9676 OLD SAUK RD
0708-163-1315-3

TOWN OF MIDDLETON
TIMOTHY T AND JULIE L BARNETT
7324 W OLD SAUK RD
0708-163-9730-3



THE PROFILE ELEVATIONS ON THIS SHEET ARE TO BE REFERRED TO ESTABLISH BACK OF SIDEWALK ELEVATIONS ONLY. SEE THE PLAN VIEW AND CROSS SECTIONS FOR THE STREET DESIGN ELEVATIONS.

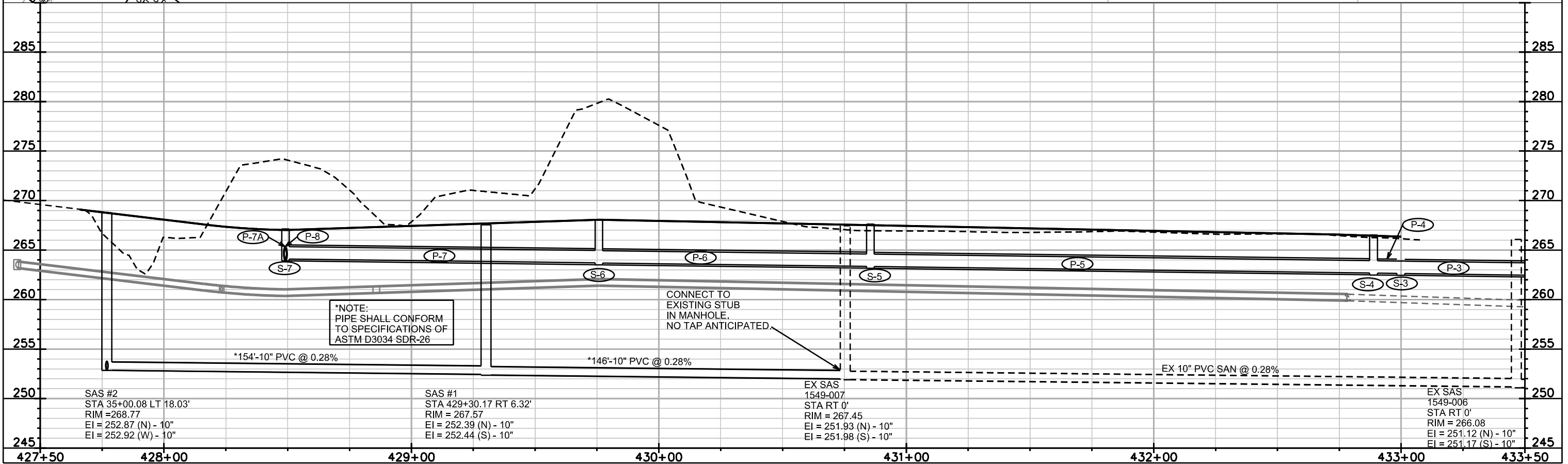
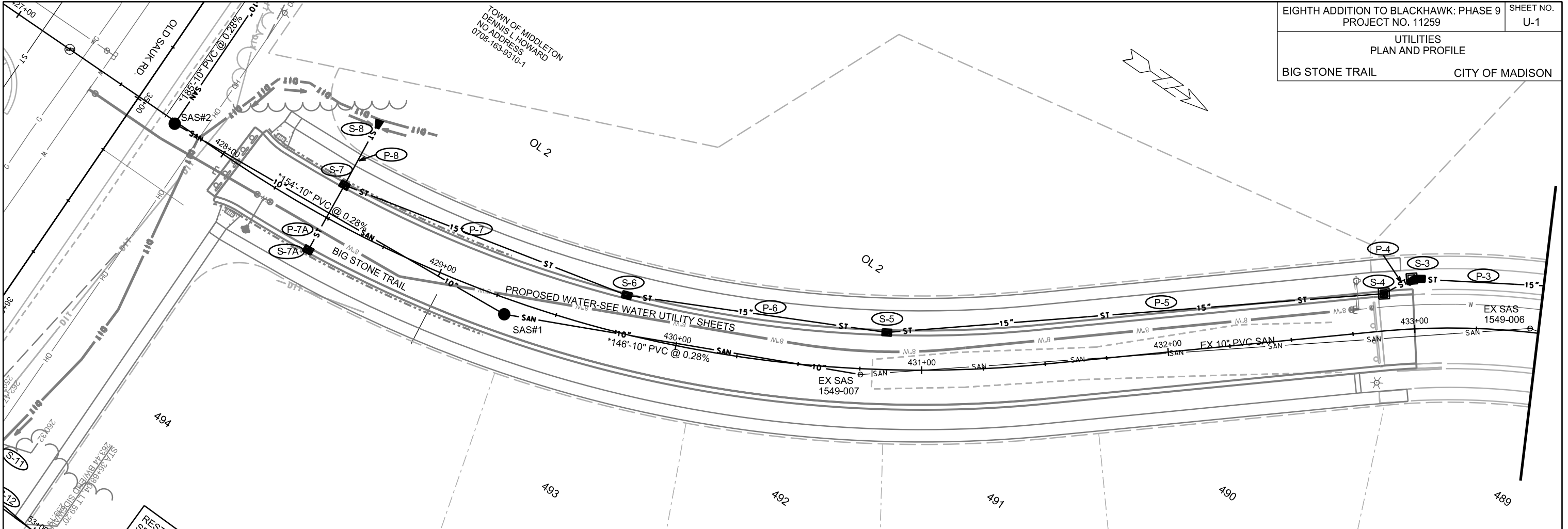
PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

TOWN OF MIDDLETON
 DENNIS L. HOWARD
 NO ADDRESS
 0708-163-9310-1



*NOTE:
 PIPE SHALL CONFORM
 TO SPECIFICATIONS OF
 ASTM D3034 SDR-26

CONNECT TO
 EXISTING STUB
 IN MANHOLE.
 NO TAP ANTICIPATED.

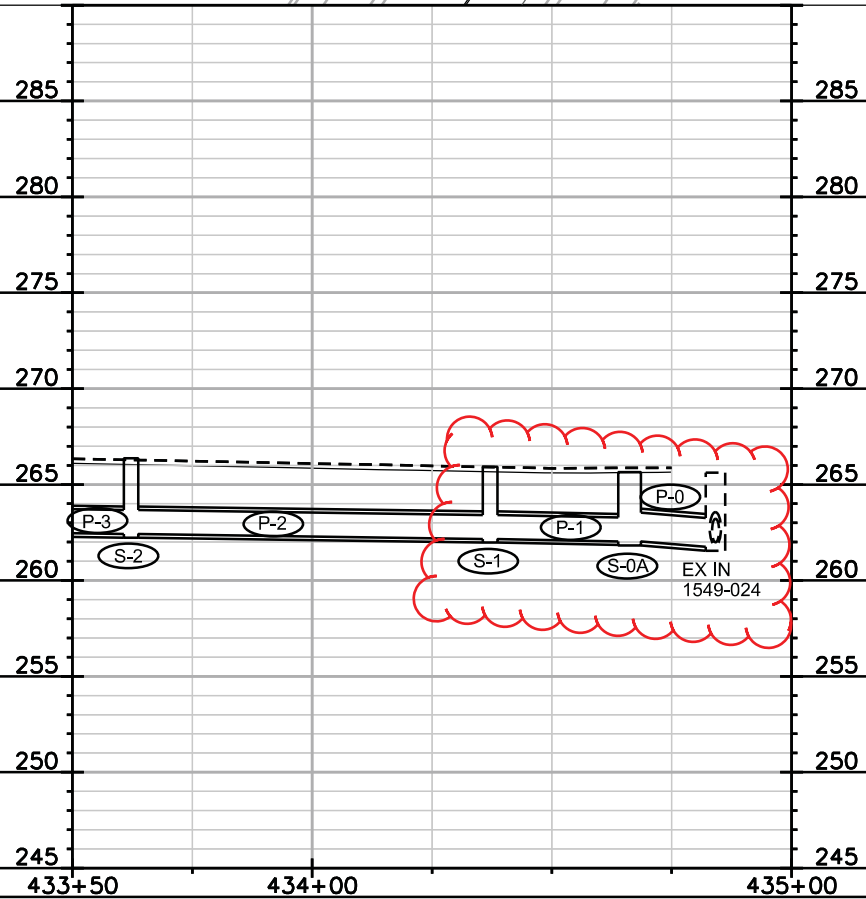
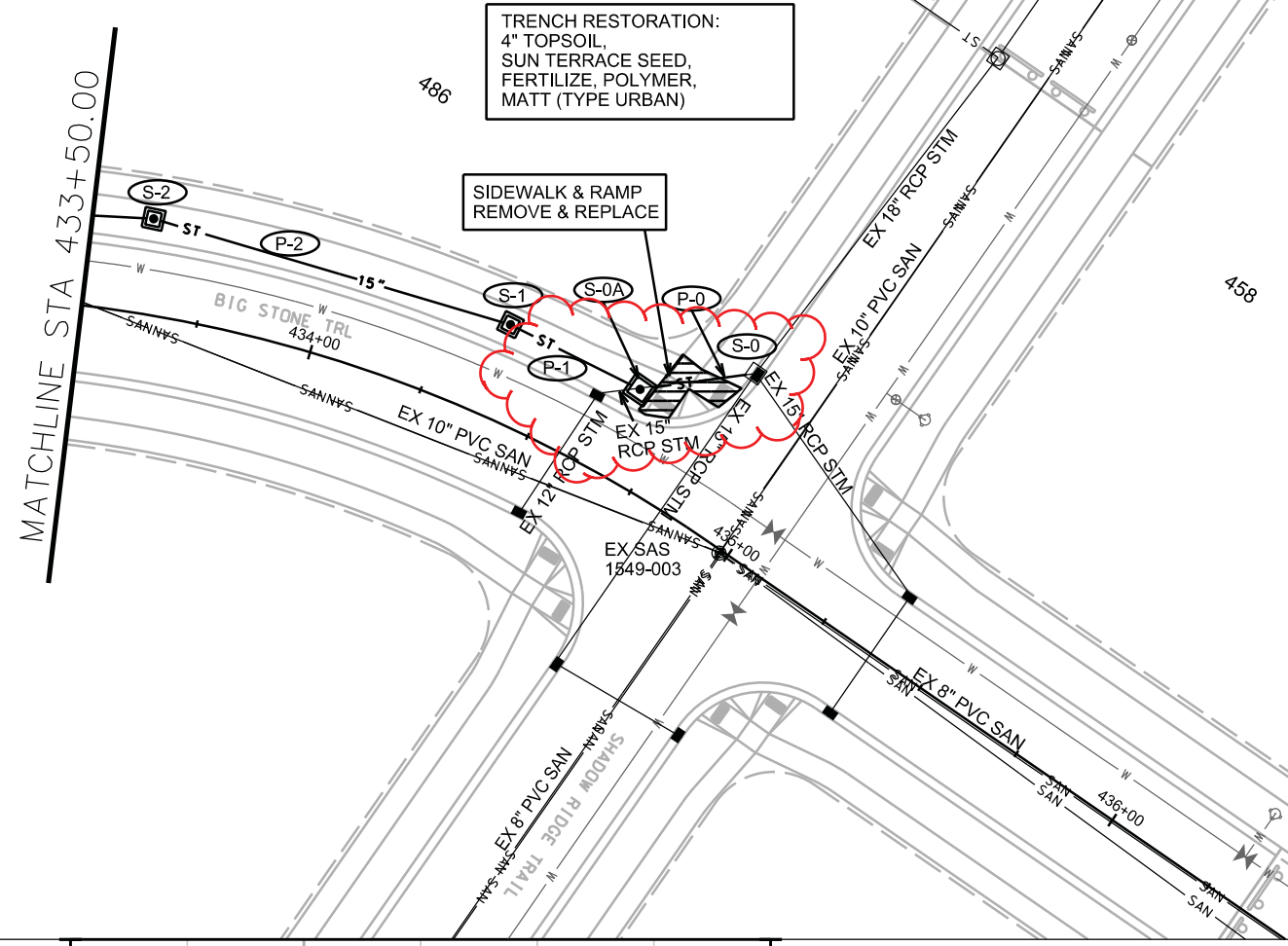
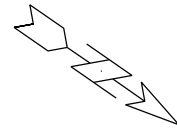
SAS #2 STA 35+00.08 LT 18.03' RIM = 268.77 EI = 252.87 (N) - 10" EI = 252.92 (W) - 10"	SAS #1 STA 429+30.17 RT 6.32' RIM = 267.57 EI = 252.39 (N) - 10" EI = 252.44 (S) - 10"	EX SAS 1549-007 STA RT 0' RIM = 267.45 EI = 251.93 (N) - 10" EI = 251.98 (S) - 10"	EX SAS 1549-006 STA RT 0' RIM = 266.08 EI = 251.12 (N) - 10" EI = 251.17 (S) - 10"
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PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



TRENCH RESTORATION:
4" TOPSOIL,
SUN TERRACE SEED,
FERTILIZE, POLYMER,
MATT (TYPE URBAN)

SIDEWALK & RAMP
REMOVE & REPLACE

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

SANITARY
PLAN AND PROFILE
OLD SAUK ROAD CITY OF MADISON

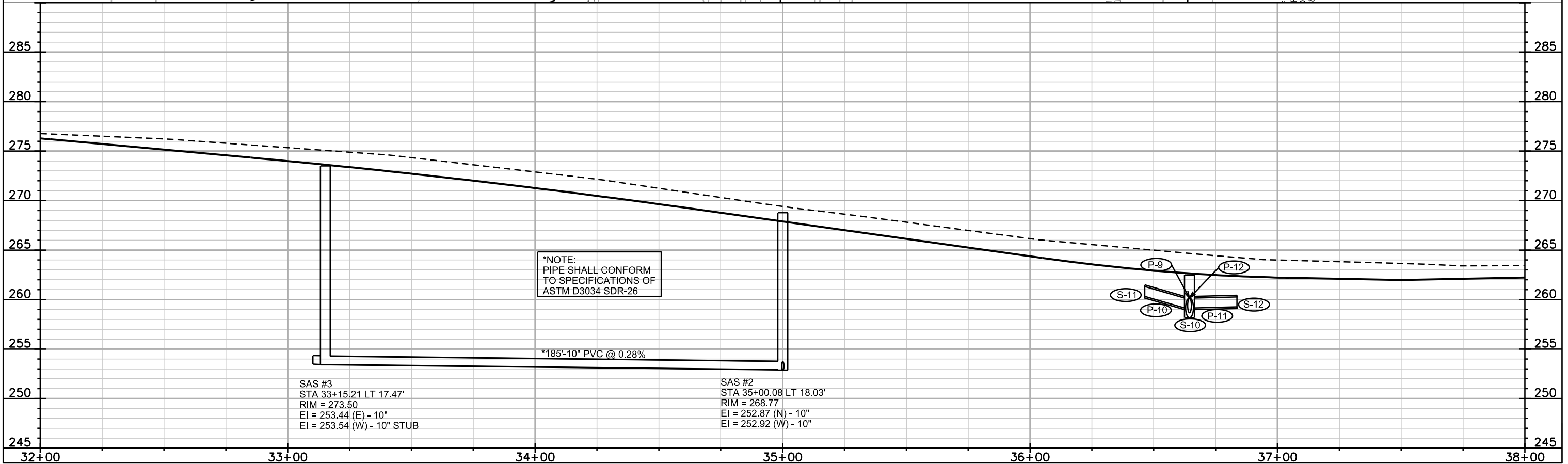
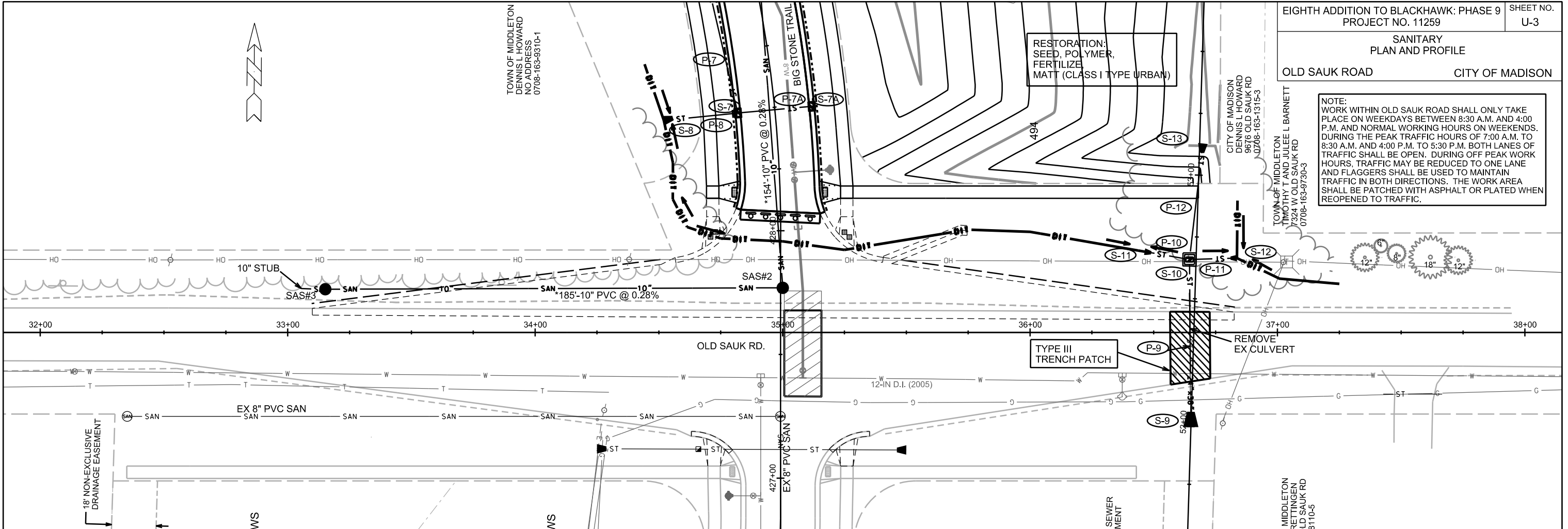
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TOWN OF MIDDLETON
TIMOTHY T AND JULIE L BARNETT
7324 W OLD SAUK RD
0708-163-9730-3

RESTORATION:
SEED, POLYMER,
FERTILIZE
MATT (CLASS I TYPE URBAN)

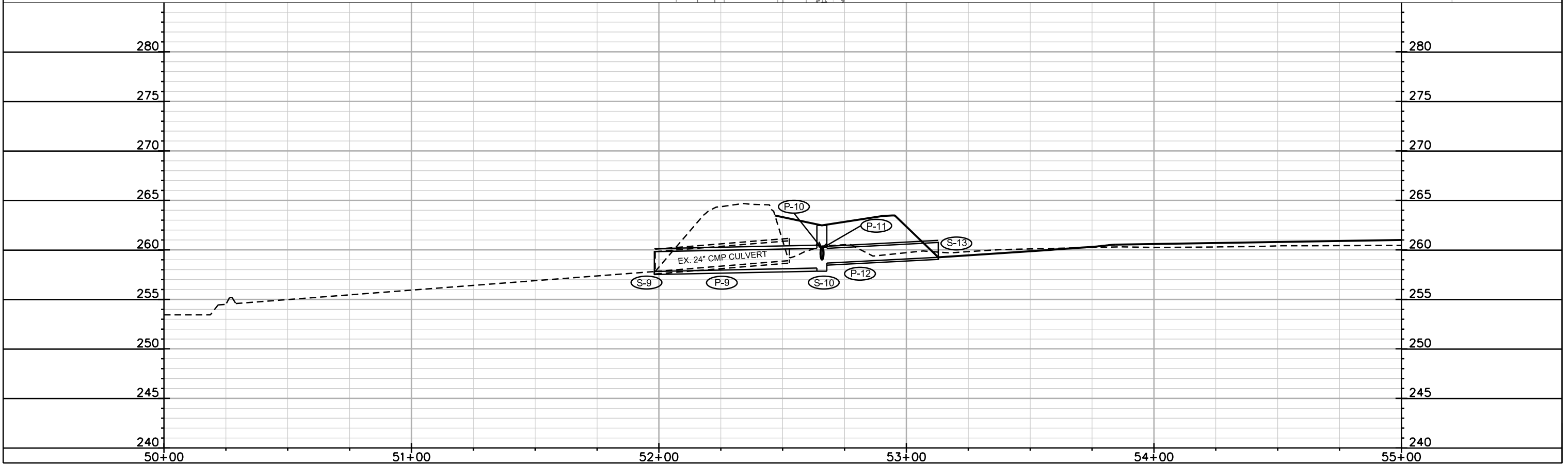
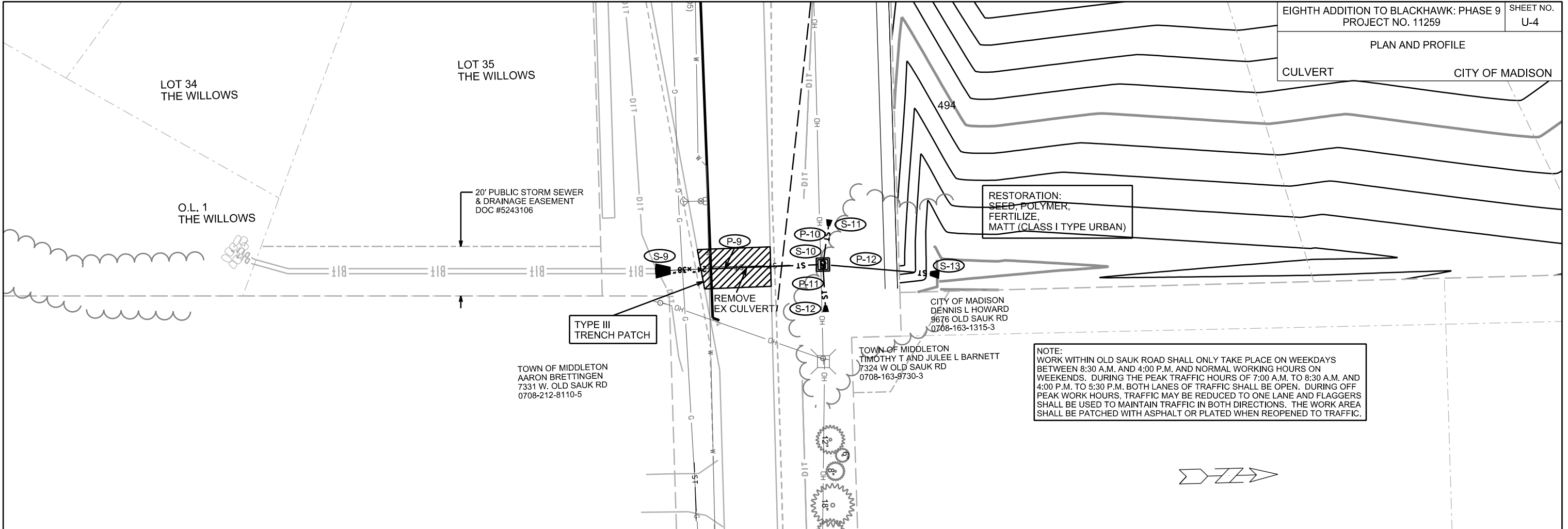


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PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

SANITARY SEWER SCHEDULE

PROPOSED SANITARY STRUCTURES

SAS NO.	STATION	LOCATION (OFFSET)	TOP OF CASTING	E.I.	DEPTH	NOTES
BIG STONE TRL						
SAS #1	429+30.17	RT-6.32	267.57	252.39	15.18	-
OLD SAUK RD						
SAS #2	35+00.08	LT-18.03	268.77	252.87	15.90	-
SAS #3	33+15.21	LT-17.47	273.50	253.44	20.06	-

PROPOSED SANITARY PIPES

FROM (DNSTM)	TO (UPSTM)	DWNSTRM E.I.	UPSTRM E.I.	PLAN LGTH (FT)	SLOPE (%)	PIPE SIZE	PVC TYPE	NOTES
BIG STONE TRL								
EX	SAS #1	251.98	252.39	146	0.28%	10"	SDR-26	-
SAS #1	SAS #2	252.44	252.87	154	0.28%	10"	SDR-26	-
OLD SAUK RD								
SAS #2	SAS #3	252.92	253.44	185	0.28%	10"	SDR-26	-

SPECIFIC NOTES

STORM SEWER SCHEDULE

* REV 10/17/16 GTF

EIGHTH ADDITION TO BLACKHAWK: PHASE 9
PROJECT NO. 11259

SHEET NO.
U-6

STORM SEWER SCHEDULE CITY OF MADISON

STRUCTURES

STRUC. NO.	STATION	LOCATION (OFFSET)	TYPE	TOP OF CASTING	E.I.	DEPTH	NOTES
* S-0	434+83.65	LT-35.68	STM TAP	-	261.76	-	TAP EX IN 1549-024
* S-0A	434+66.05	LT-19.94	4X4 SAS	265.65	262.03	3.62	FP; W/ R-1550-0054; (1)
S-1	434+37.11	LT-20.23	3X3 SAS	265.92	262.16	3.76	FP; W/ R-1550
S-2	433+62.23	LT-20.48	3X3 SAS	266.37	262.42	3.95	FP; W/ R-1550
S-3	433+01.55	LT-19.85	FIELD BEND	-	262.64	-	-
S-4	432+88.83	LT-15.50	3X3 SAS	266.52	262.68	3.84	FP; W/ R-3067-7004-V
S-5	430+85.48	LT-15.50	H INLET	267.62	263.35	4.27	LP; FP; W/ R-3067-7004-VB
S-6	429+76.00	LT-15.50	H INLET	268.07	263.70	4.37	FP; W/ R-3067-7004-V
S-7	428+49.00	LT-15.50	H INLET	267.13	264.10	3.03	LP; FP; W/ R-3067-7004-VB
S-7A	428+49.00	RT-15.50	H INLET	267.13	264.25	2.88	LP; FP; W/ R-3067-7004-VB
S-8	428+49.00	LT-45.06	12" APRON END	-	264.30	-	-
S-9	51+98.18	CL	24"X38" HERCP APRON END	-	257.82	-	-
S-10	52+65.83	LT-2.67	4X4 SAS	262.47	258.16	4.31	FP; W/ R-1878-B7G
S-11	52+68.00	LT-18.00	12" RCP AE	-	260.32	-	-
S-12	52+67.00	RT-16.50	12" RCP AE	-	259.27	-	-
S-13	53+12.88	RT-1.28	18" RCP AE	-	259.25	-	-

PIPES

PIPE NO.	FROM (DNSTM)	TO (UPSTM)	PLAN LGTH (FT)	DISCH. E.I.	INLET E.I.	SLOPE (%)	PIPE SIZE	TYPE	NOTES
* P-0	S-0	S-0A	25	261.76	262.03	1.08%	18"	TYPE II STORM	REMOVE EX 15" STM
* P-1	S-0A	S-1	31	262.03	262.16	0.42%	15"	RCP	NCM
P-2	S-1	S-2	80	262.16	262.42	0.32%	15"	RCP	-
P-3	S-2	S-3	67	262.42	262.64	0.33%	15"	RCP	-
P-4	S-3	S-4	13	262.64	262.68	0.31%	15"	RCP	-
P-5	S-4	S-5	202	262.68	263.35	0.33%	15"	RCP	-
P-6	S-5	S-6	106	263.35	263.70	0.33%	15"	RCP	-
P-7	S-6	S-7	122	263.70	264.10	0.33%	15"	RCP	-
P-7A	S-7	S-7A	29	264.10	264.25	0.52%	12"	RCP	NCM
P-8	S-7	S-8	30	264.10	264.30	0.67%	12"	RCP	NCM
P-9	S-9	S-10	68	257.82	258.16	0.50%	24"X38"	HERCP	-
P-10	S-10	S-11	18	259.16	260.32	6.44%	12"	RCP	-
P-11	S-10	S-12	19	259.16	259.27	0.58%	12"	RCP	-
P-12	S-10	S-13	47	258.66	259.25	1.26%	18"	RCP	-

STANDARD NOTES:

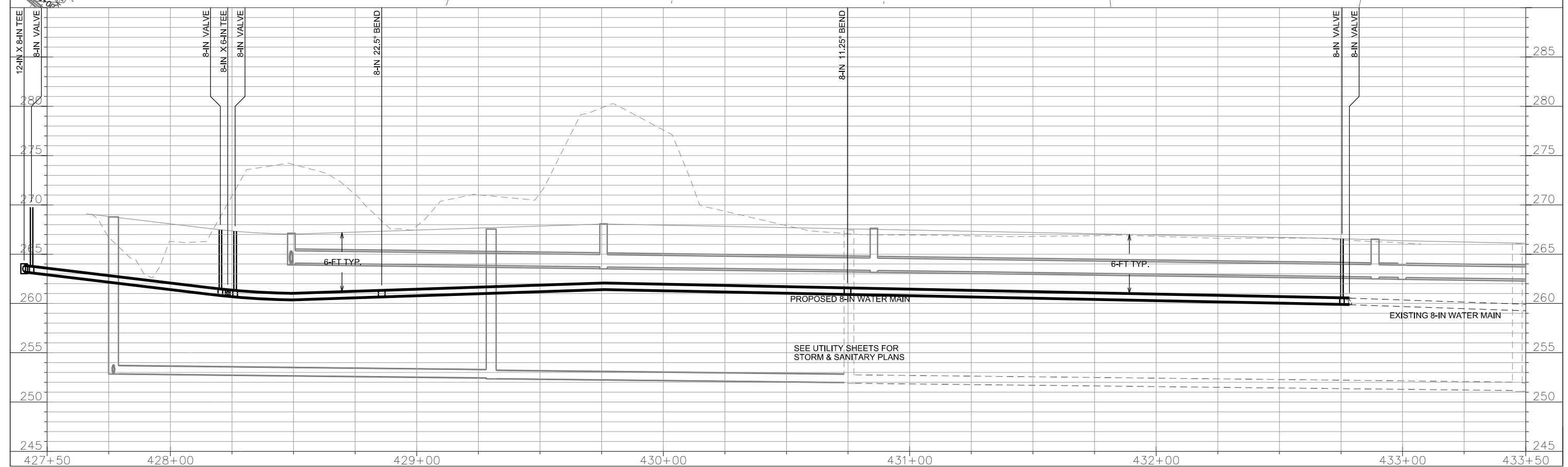
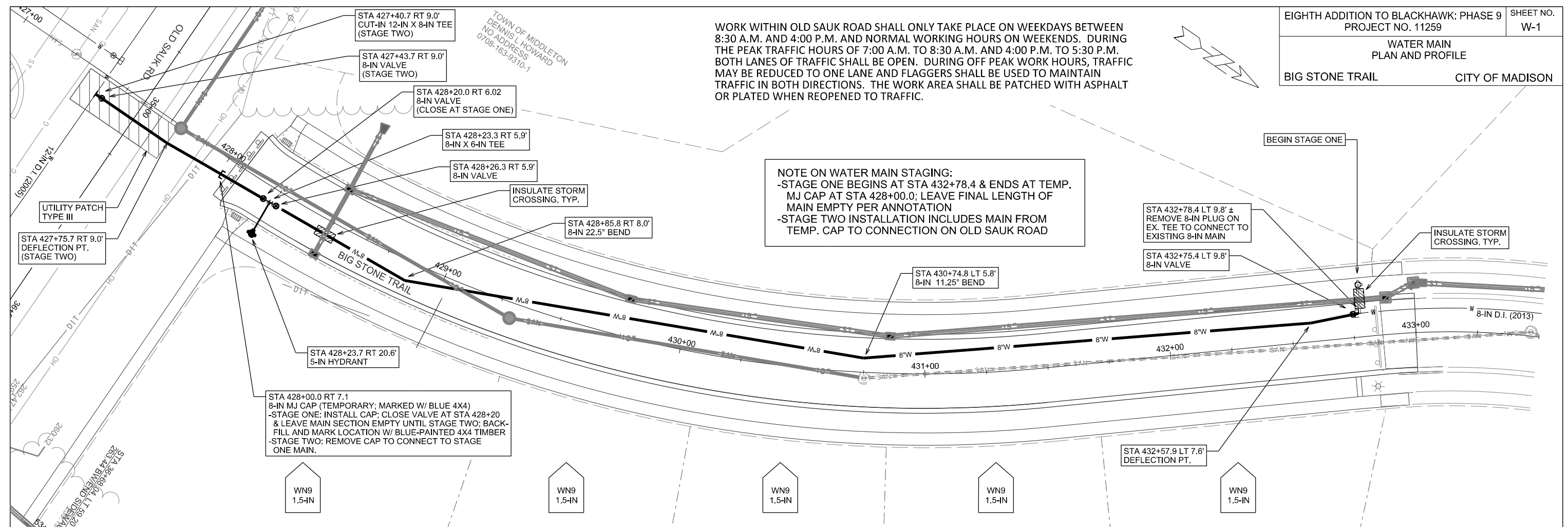
- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD POURED STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES
- APPROXIMATE DISCHARGE E.I. GIVEN, ADJUST E.I. AND PIPE SLOPE IN THE FIELD.
- TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SAS'S.
- TOP OF CONCRETE ROOF (TR) IS 1.25' BELOW TOP OF CASTING UNLESS OTHERWISE NOTED.
- ALL REINFORCED CONCRETE PIPES TO BE CLASS III UNLESS OTHERWISE NOTED.
- SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.
- ALL STRUCTURES CALLED OUT AS FIELD POURED SHALL BE FIELD POURED. ALL OTHER STRUCTURES (NOT INDICATED AS FIELD POURED) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL IF PRECAST STRUCTURES ARE PREFERRED. CONTACT ELIA E. ACOSTA OF CITY ENGINEERING AT (608) 266-4096 FOR PRECAST APPROVALS, OR FAX SHOP DRAWINGS TO (608)264-9275.

SPECIFIC NOTES

- * (1) CONNECT EX 15" PIPE TO SOUTHEAST

WORK WITHIN OLD SAUK ROAD SHALL ONLY TAKE PLACE ON WEEKDAYS BETWEEN 8:30 A.M. AND 4:00 P.M. AND NORMAL WORKING HOURS ON WEEKENDS. DURING THE PEAK TRAFFIC HOURS OF 7:00 A.M. TO 8:30 A.M. AND 4:00 P.M. TO 5:30 P.M. BOTH LANES OF TRAFFIC SHALL BE OPEN. DURING OFF PEAK WORK HOURS, TRAFFIC MAY BE REDUCED TO ONE LANE AND FLAGGERS SHALL BE USED TO MAINTAIN TRAFFIC IN BOTH DIRECTIONS. THE WORK AREA SHALL BE PATCHED WITH ASPHALT OR PLATED WHEN REOPENED TO TRAFFIC.

NOTE ON WATER MAIN STAGING:
-STAGE ONE BEGINS AT STA 432+78.4 & ENDS AT TEMP. MJ CAP AT STA 428+00.0; LEAVE FINAL LENGTH OF MAIN EMPTY PER ANNOTATION
-STAGE TWO INSTALLATION INCLUDES MAIN FROM TEMP. CAP TO CONNECTION ON OLD SAUK ROAD



CONSTRUCTION NOTES:

1. CONSTRUCT NEW WATER MAIN 6.0' BELOW FINISHED GRADE, UNLESS OTHERWISE NOTED. INSULATE MAIN WITH POLYSTYRENE BOARD AT UTILITY CROSSINGS OR OTHER AREAS IDENTIFIED BY ENGINEER AS HAVING INADEQUATE COVER.
2. VERIFY SIZE OF EXISTING WATER SERVICES AND RECONNECT SERVICES AS INDICATED.
3. MINIMIZE DISRUPTION OF SERVICE TO EXISTING CUSTOMERS. NOTIFY PER CONTRACT REQUIREMENTS OF ANY PLANNED WATER OUTAGE.
4. THE EXISTING UTILITIES SHOWN ON THIS PLAN REPRESENT THE BEST INFORMATION AVAILABLE TO THE WATER UTILITY AT THE TIME OF PLAN PREPARATION. CONTRACTOR IS RESPONSIBLE FOR HAVING EACH UTILITY LOCATED PRIOR TO COMMENCING WORK.

- WN1 REPLACE THE EXISTING LEAD SERVICE WITH A NEW COPPER SERVICE.
- WN2 EXTEND AND RECONNECT THE EXISTING COPPER SERVICE TO THE NEW WATER MAIN.
- WN3 EXISTING SERVICE TO BE ABANDONED WHEN THE WATER MAIN IS CUT OFF.
- WN4 DISCONNECT FROM THE OLD WATER MAIN AND RECONNECT THE EXISTING COPPER WATER SERVICE LATERAL TO THE NEW WATER MAIN.
- WN5 RELOCATE THE EXISTING FIRE HYDRANT.
- WN6 ABANDON WATER VALVE ACCESS STRUCTURE.
- WN7 FURNISH AND INSTALL THE NEW TOP SECTION FOR THE WATER ACCESS STRUCTURE.
- WN8 ABANDON THE VALVE BOX.
- WN9 FURNISH THE DITCH, COMPACTION, AND ALL MATERIALS AND LABOR FOR THE INSTALLATION OF NEW SERVICE LATERAL.
- WN10 REMOVE AND SALVAGE EXISTING HYDRANT
- WN11 REPLACE THE EXISTING COPPER SERVICE WITH A COPPER SERVICE
- WN20+ SEE WATER IMPACT PLAN FOR CONNECTION POINT ISOLATION AND WATER SHUT-OFF NOTIFICATION INFORMATION.

ESTIMATE OF MATERIALS SUPPLIED BY CONTRACTOR:

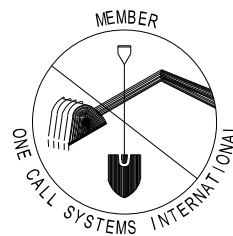
** ESTIMATE OF MATERIALS IS FOR INFORMATION ONLY. ENGINEER DOES NOT GUARANTEE ACCURACY OF MATERIAL TAKE-OFF.*

- | | |
|----------------------------|---|
| 15-FT - 6-IN PIPE | 1 - 8-IN 22.5° BEND |
| 540-FT - 8-IN PIPE | 1 - HYDRANT |
| 5-FT - 12-IN PIPE (CUT-IN) | 8-FT - 2-IN STYROFOAM INSULATION |
| 4 - 8-IN VALVE & BOX | 670-FT - POLY WRAP |
| 1 - 8-IN X 6-IN TEE | 1.5-IN COPPER (AS REQ'D) |
| 1 - 12-IN X 8-IN TEE | 1 - ~10-FT BLUE-PAINTED 4 X 4 (STAGE 1/2 LOCATION MARKER) |
| 1 - 8-IN MJ CAP (TEMP.) | |
| 1 - 8-IN 11.25° BEND | |

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

CALL DIGGERS HOTLINE
TOLL FREE
811 OR 1-800-242-8511
FAX-A-LOCATE 1-800-338-3860
TDD (FOR HEARING IMPAIRED) 1-800-542-2289

WIS. STATUTE 182.0175 (1974) REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE.

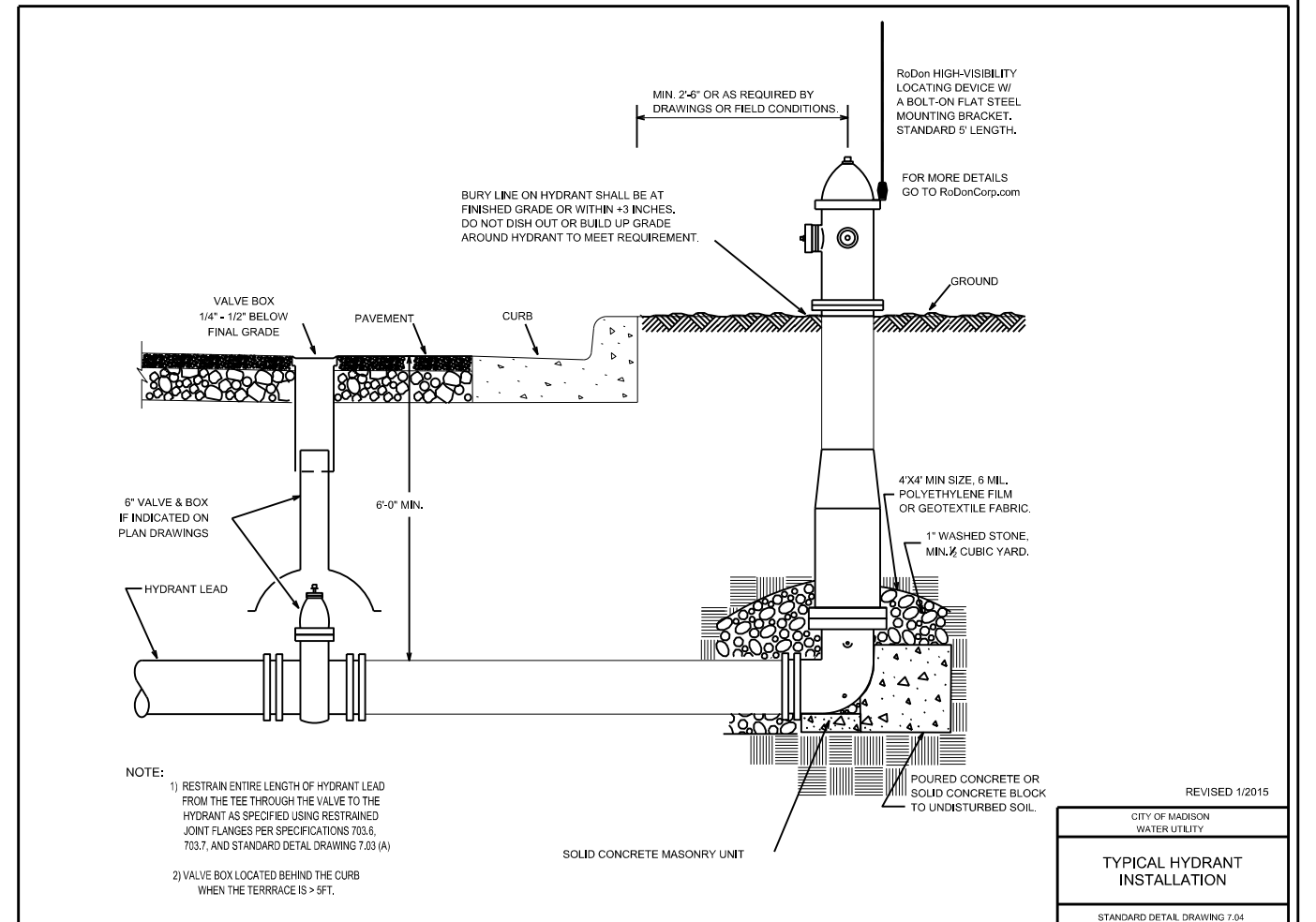
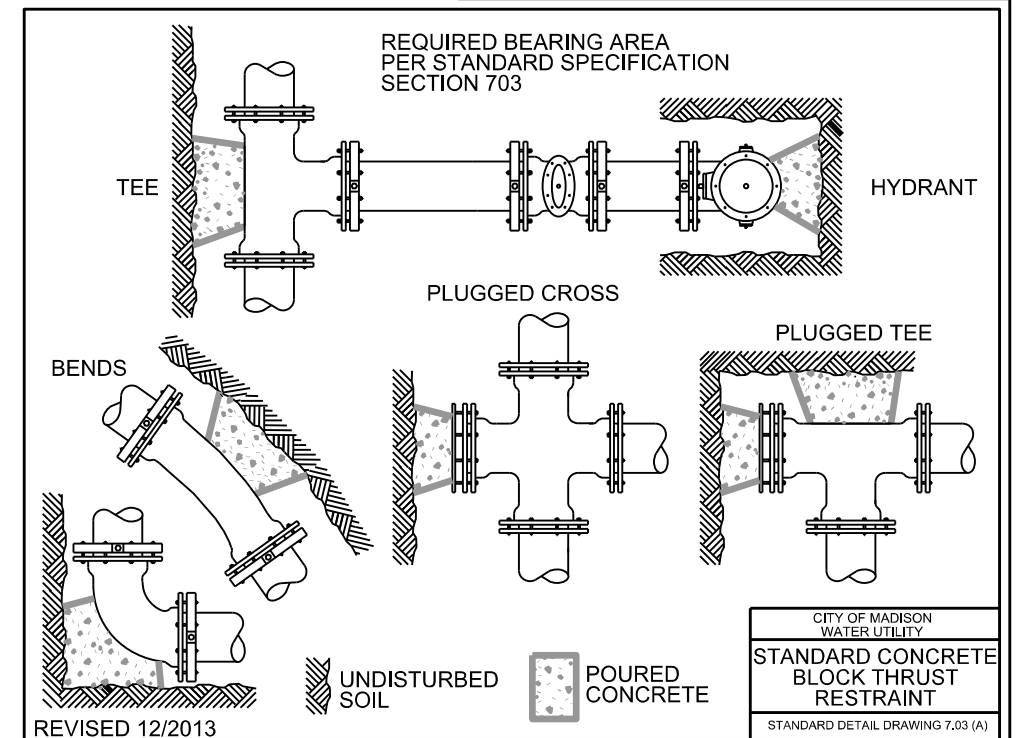


ESTIMATE OF MATERIALS SALVAGED:

** ESTIMATE OF MATERIALS IS FOR INFORMATION ONLY. ENGINEER DOES NOT GUARANTEE ACCURACY OF MATERIAL TAKE-OFF.*

NONE

DISCLAIMER NOTE:
UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXACT HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UNDERGROUND AND OVERHEAD UTILITIES PRIOR TO COMMENCING WORK.



CROSS SECTIONS

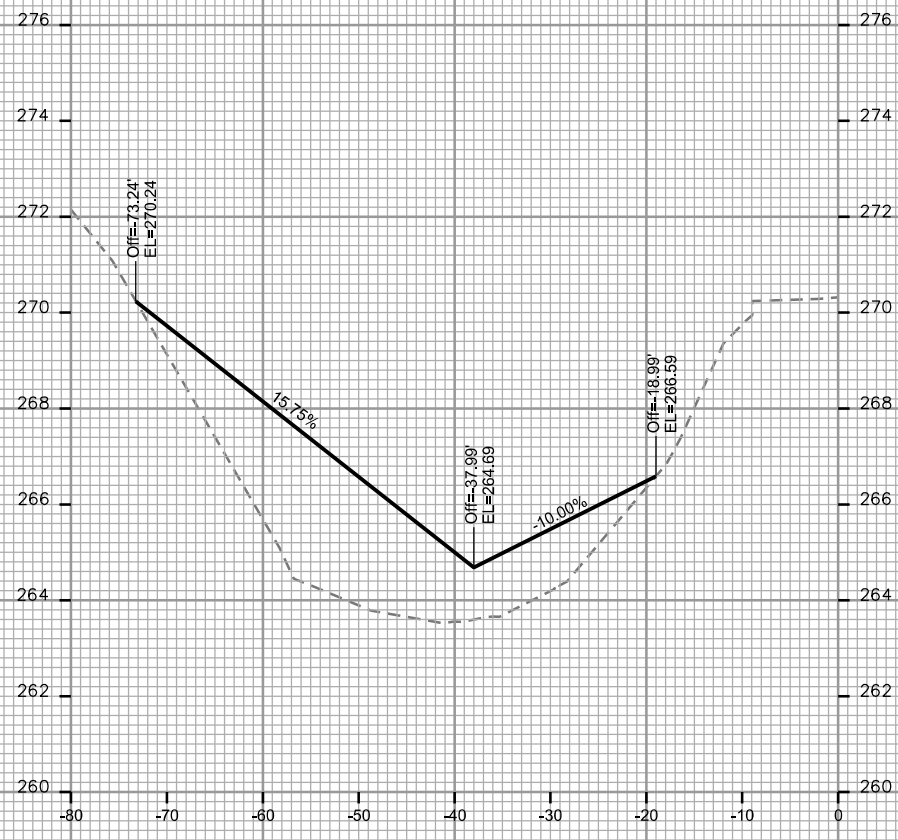
STAGE 1: OLD SAUK ROAD CITY OF MADISON

PLOT SCALE: _____

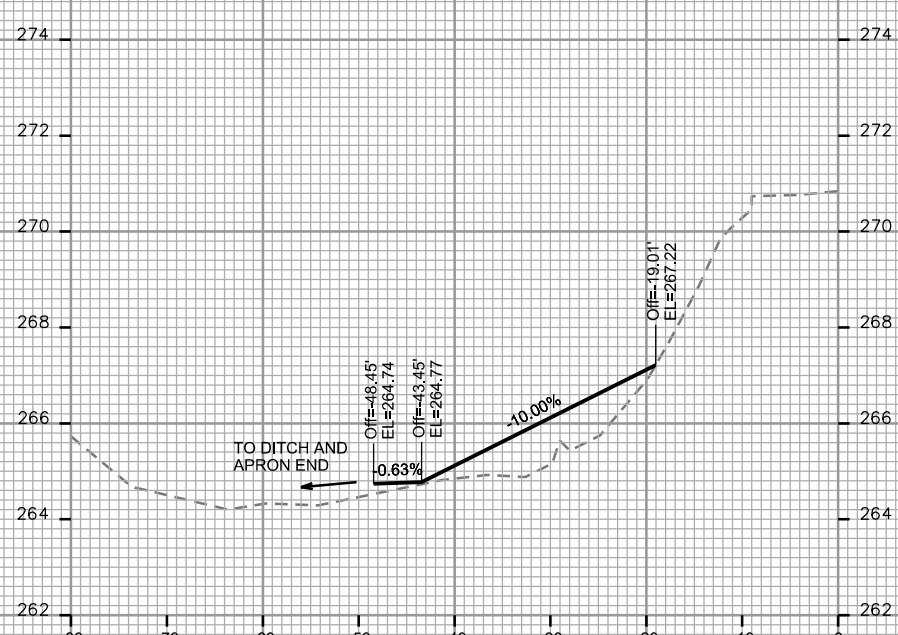
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REV. DATE: _____

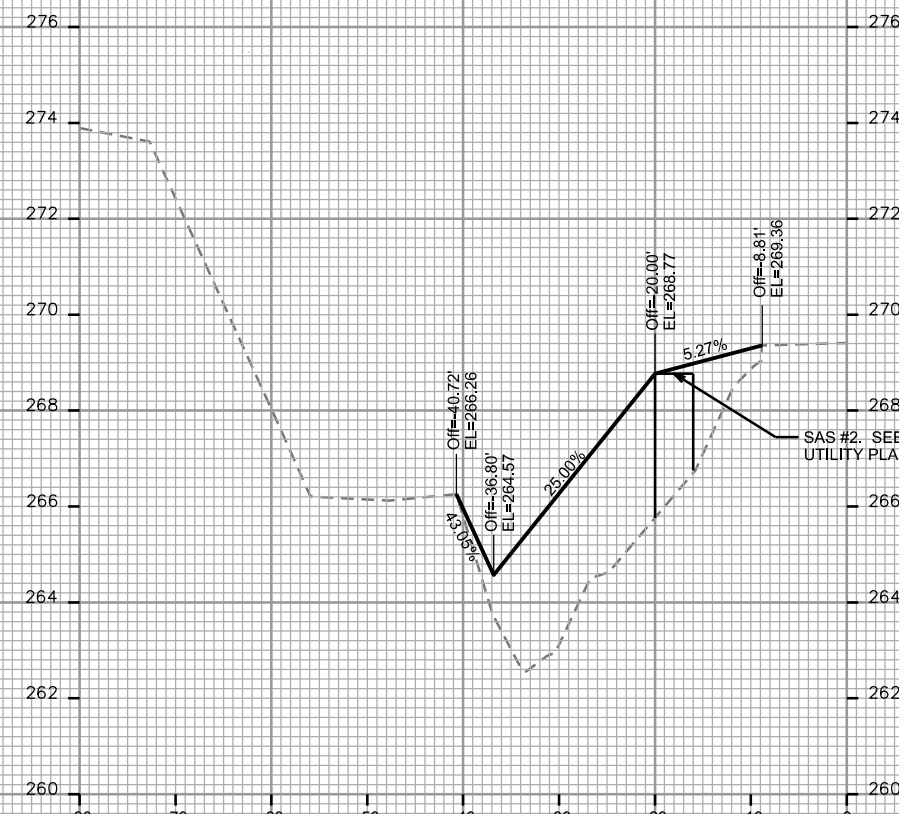
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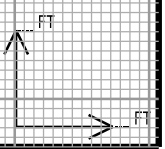
STA. 34+75



STA. 34+61



STA. 35+00



CROSS SECTIONS

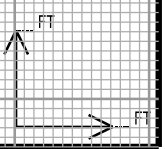
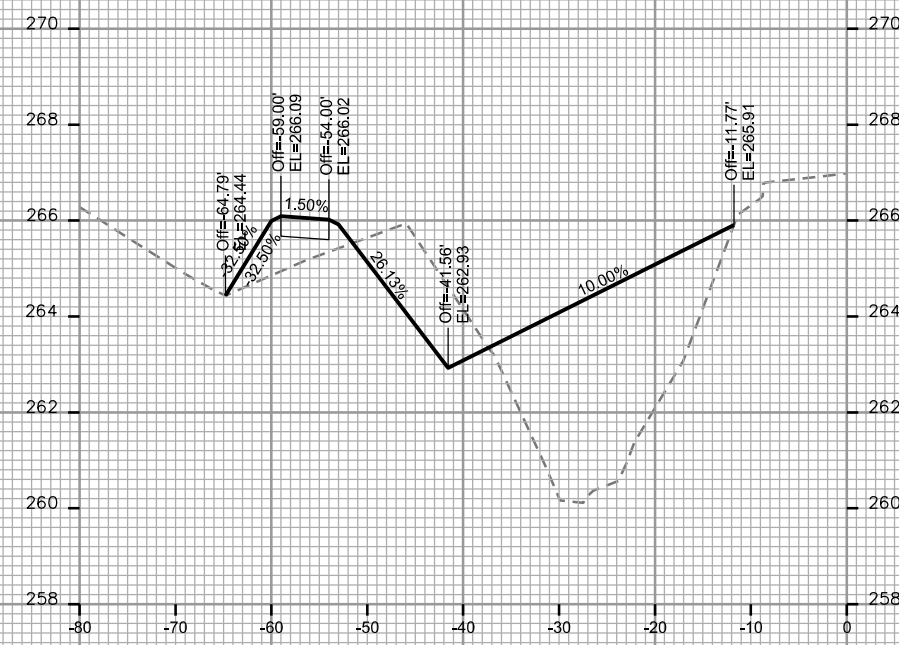
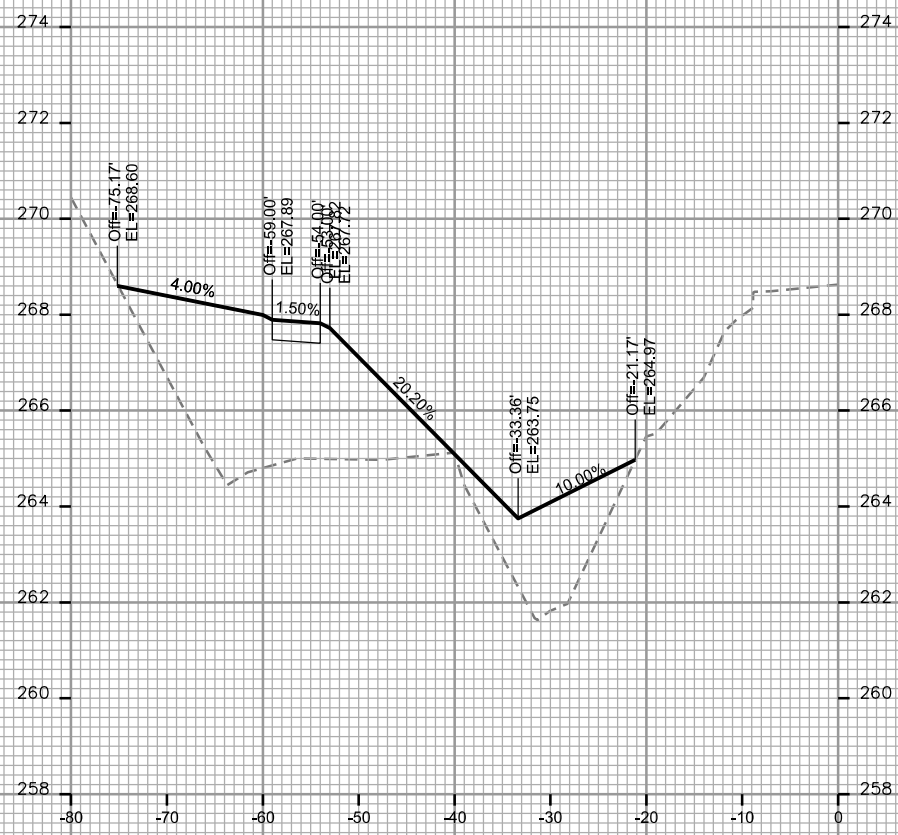
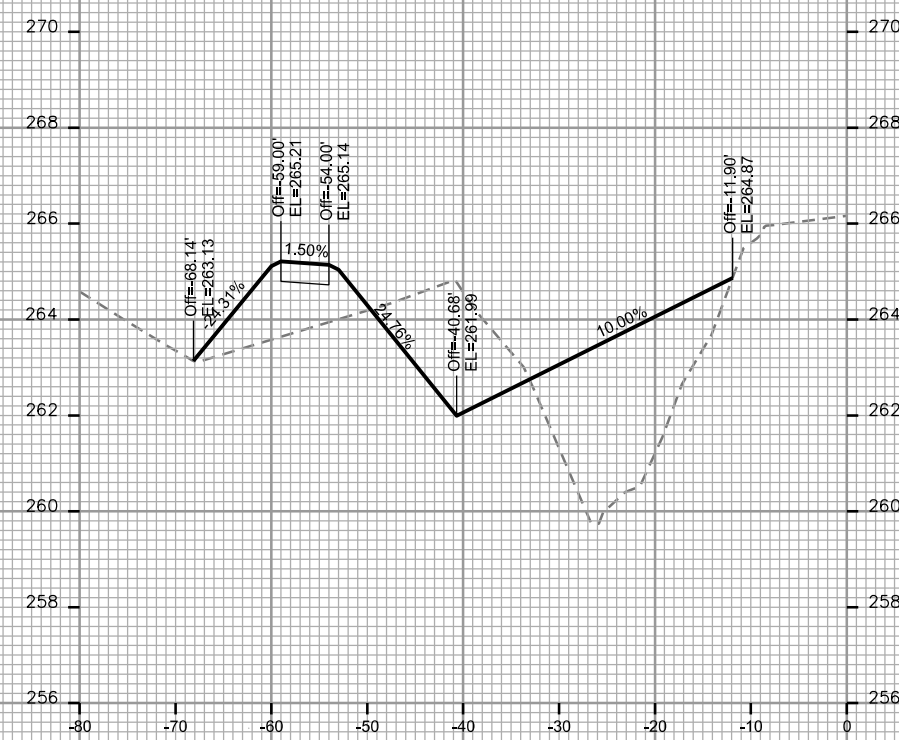
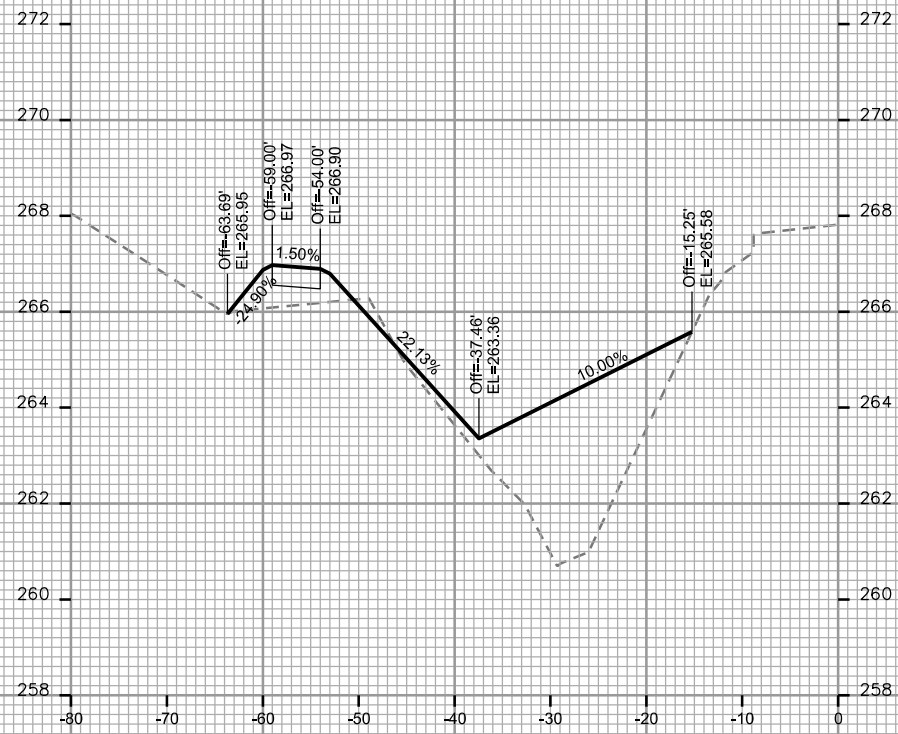
STAGE 1: OLD SAUK ROAD CITY OF MADISON

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON STREETS DIVISION



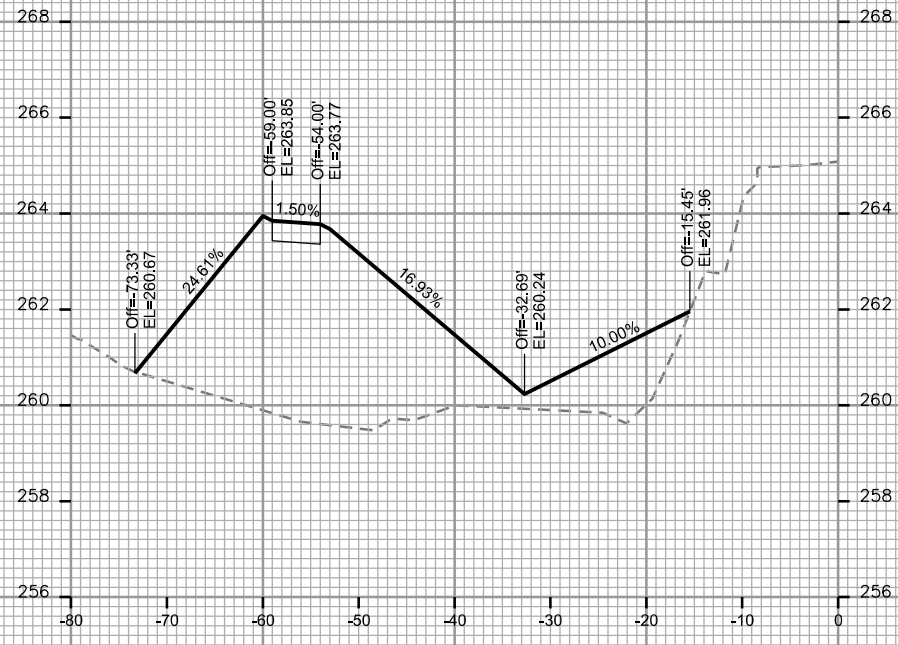
CROSS SECTIONS
STAGE 1: OLD SAUK ROAD CITY OF MADISON

PLOT SCALE: _____

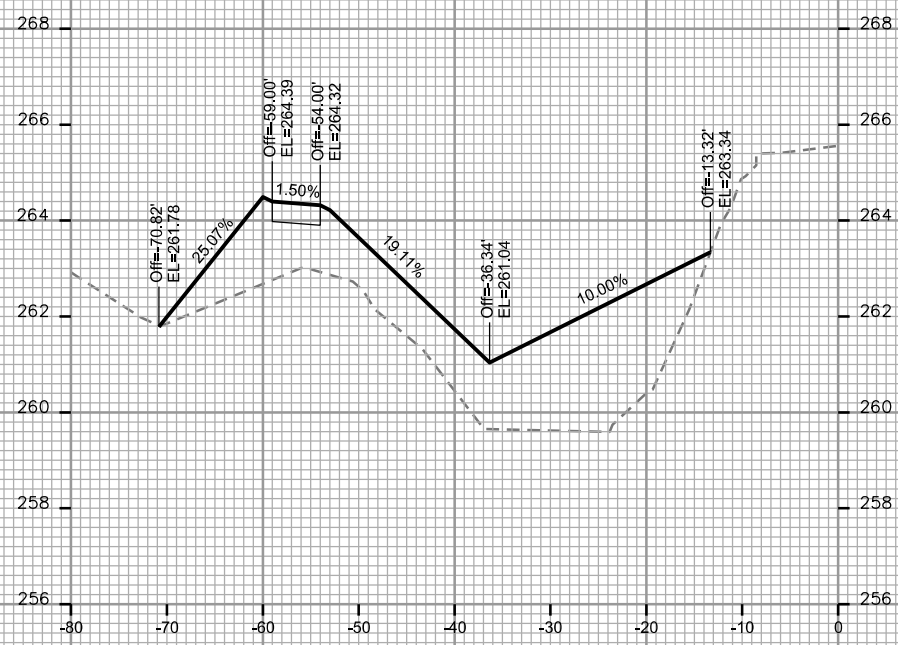
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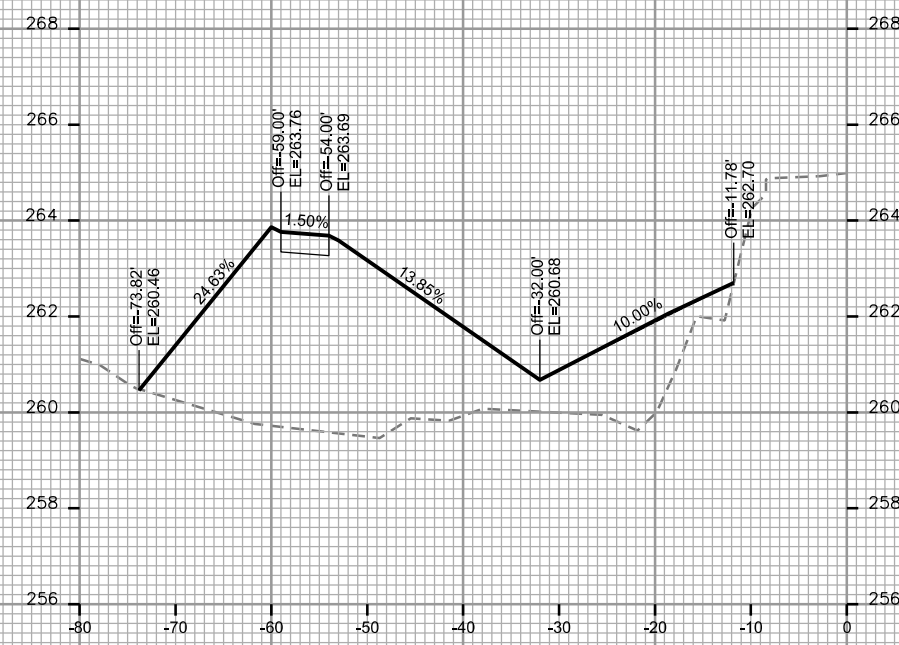
ORIGINATOR: CITY OF MADISON STREETS DIVISION



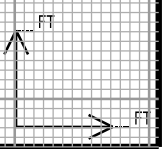
STA. 36+46



STA. 36+25



STA. 36+50



CROSS SECTIONS

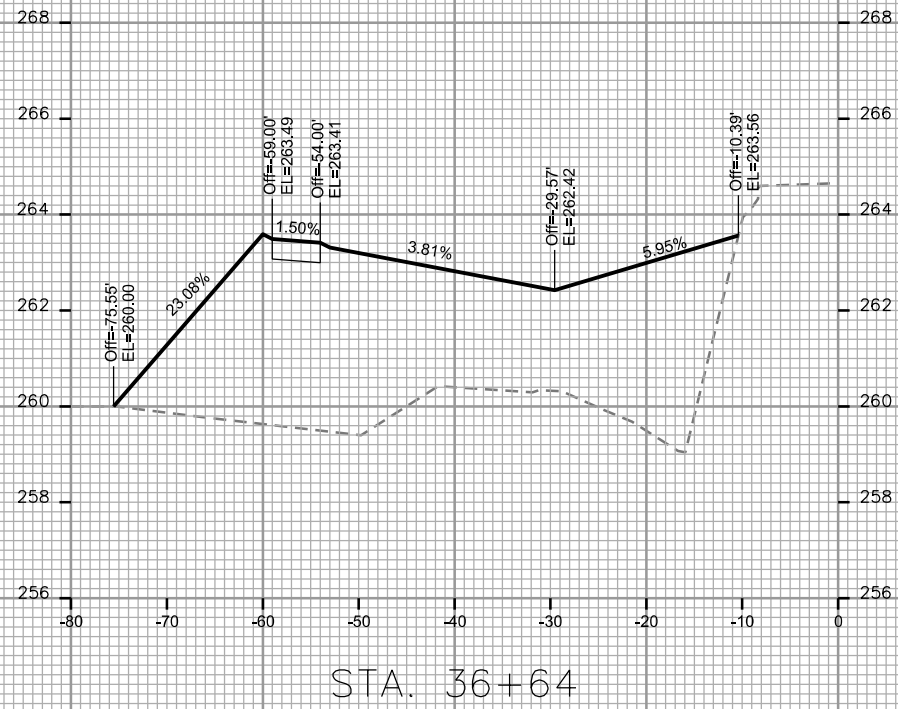
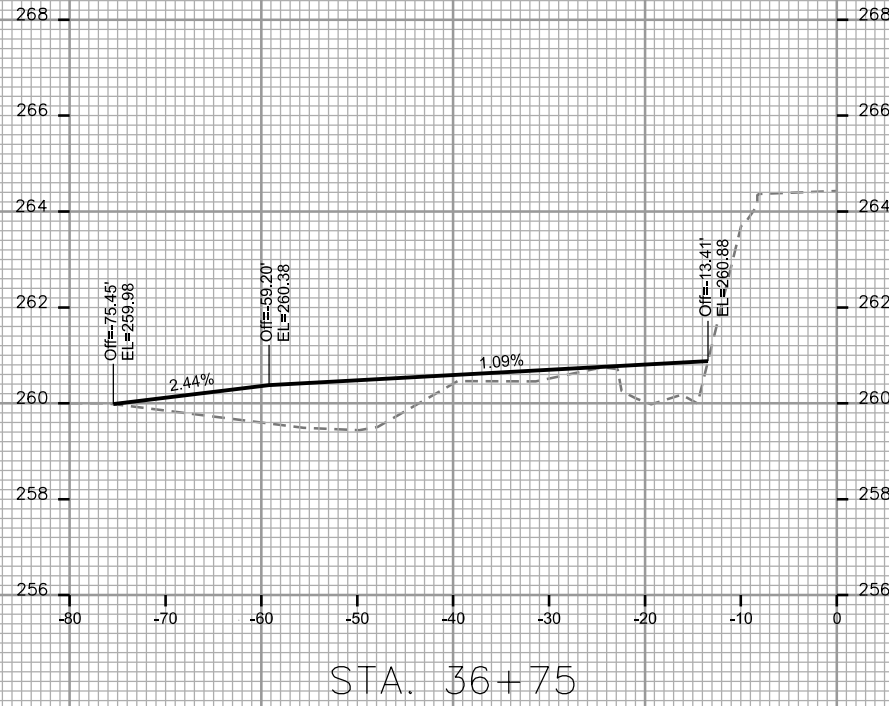
STAGE 1: OLD SAUK ROAD CITY OF MADISON

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PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

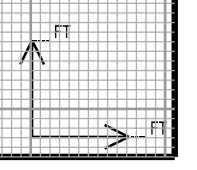
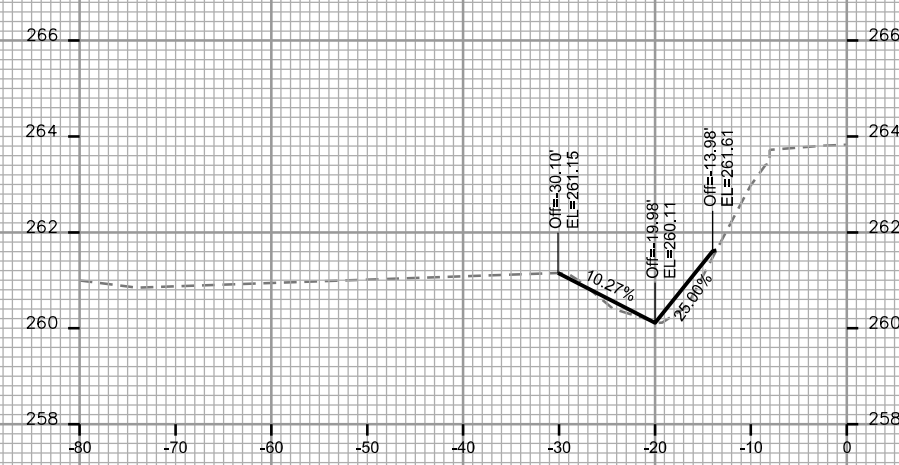
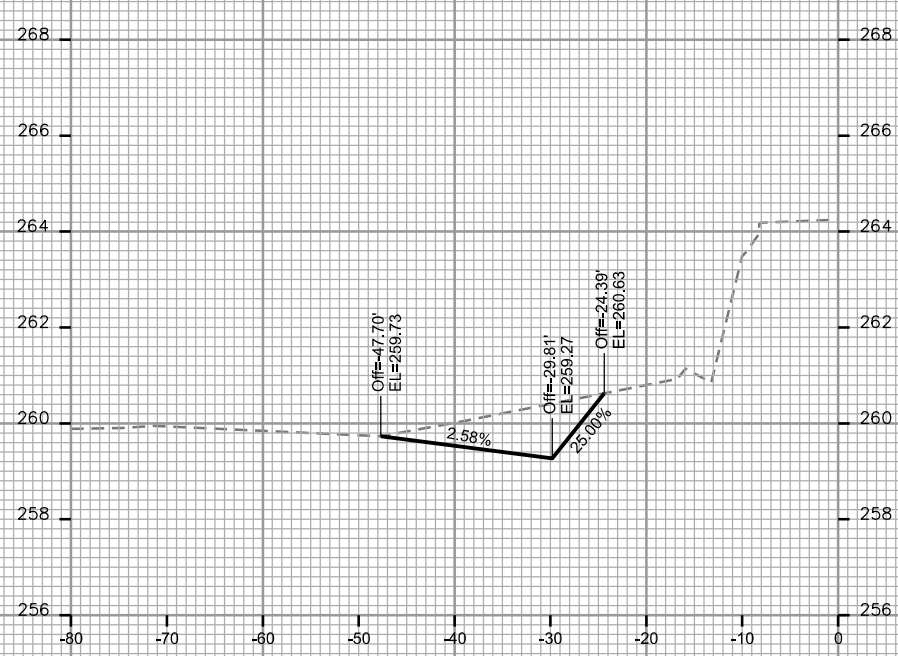
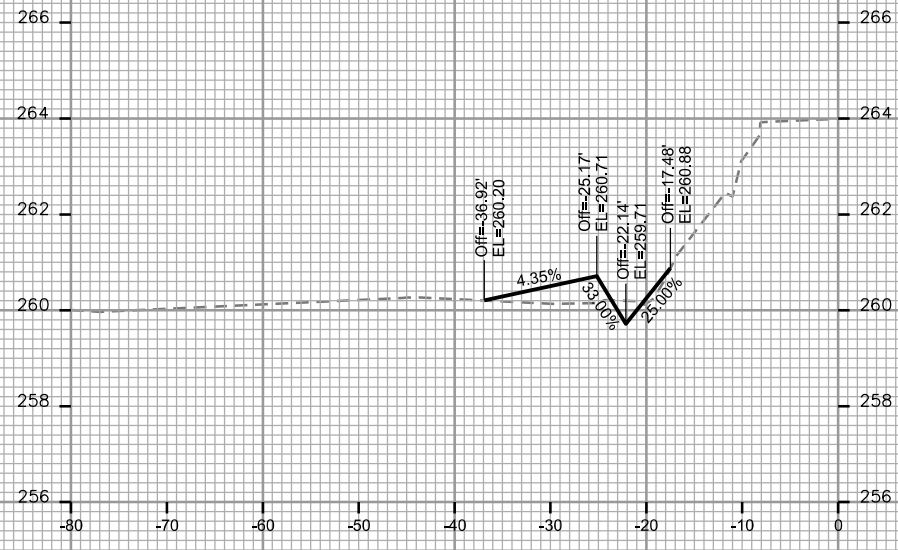
STAGE 1: OLD SAUK ROAD CITY OF MADISON

PLOT SCALE: _____

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REV. DATE: _____

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



CROSS SECTIONS

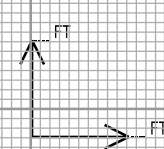
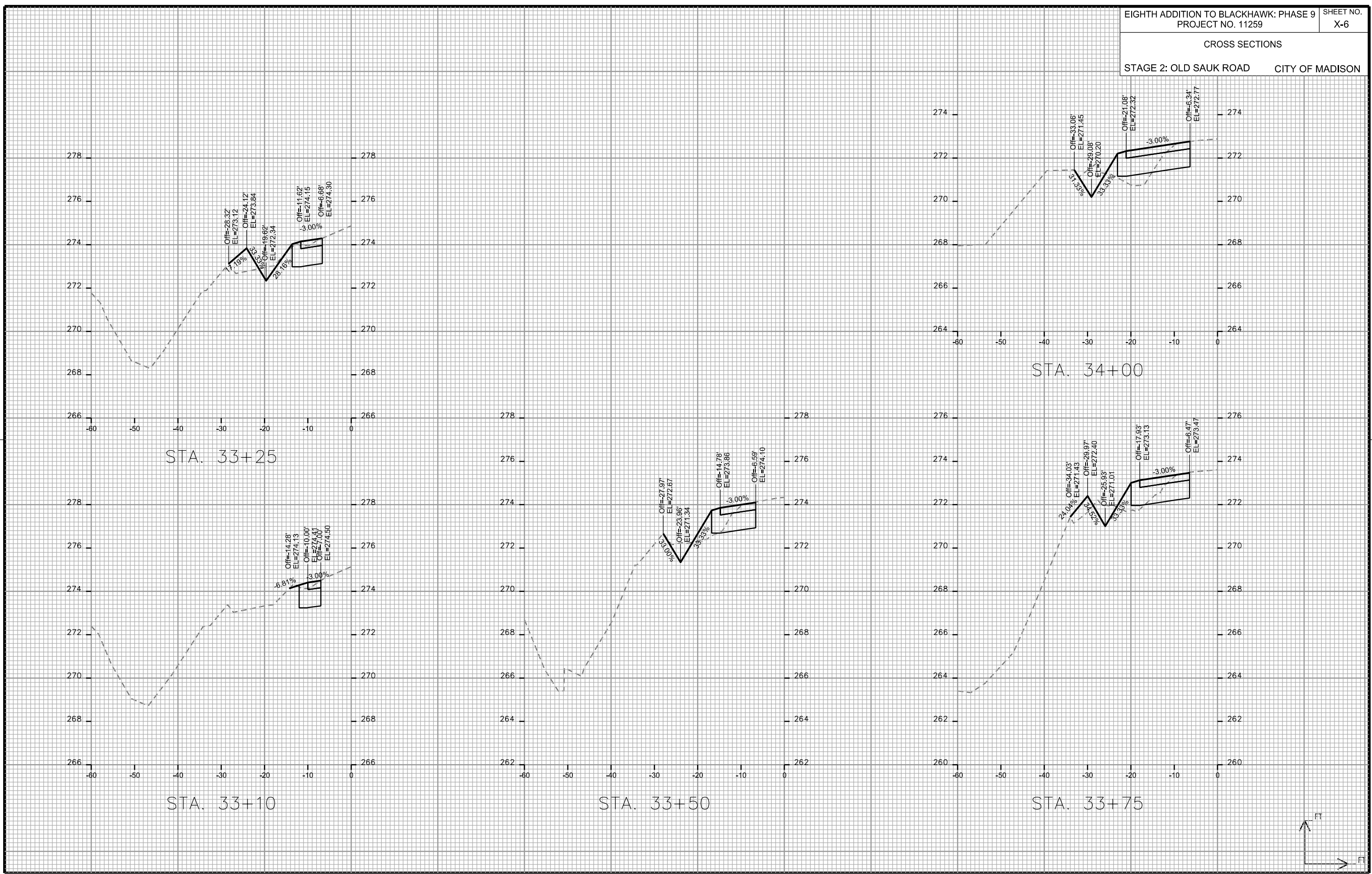
STAGE 2: OLD SAUK ROAD CITY OF MADISON

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

ORIGINATOR: CITY OF MADISON - STREETS DIVISION



CROSS SECTIONS

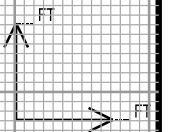
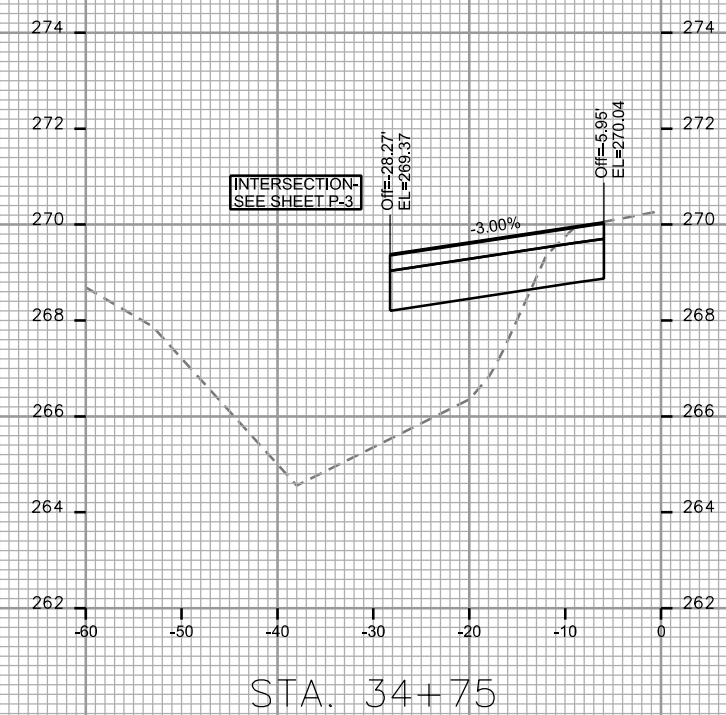
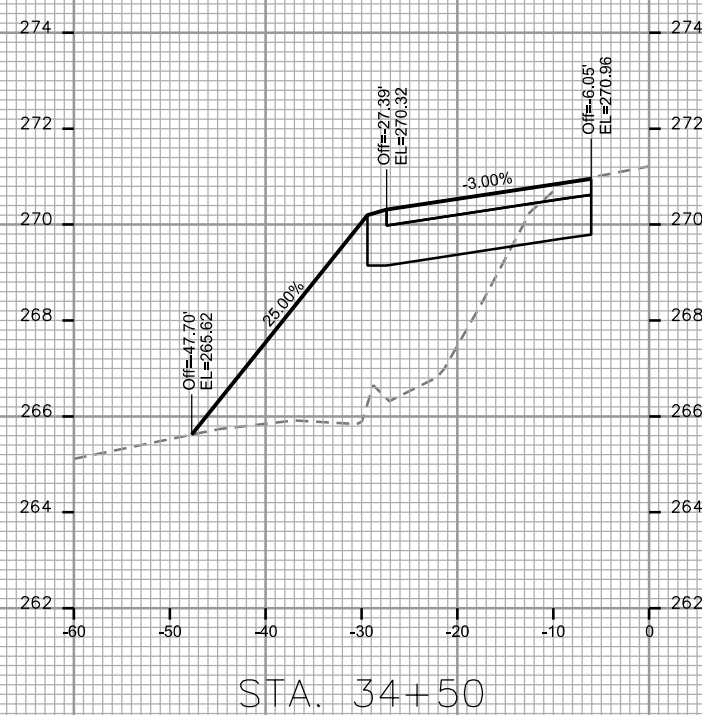
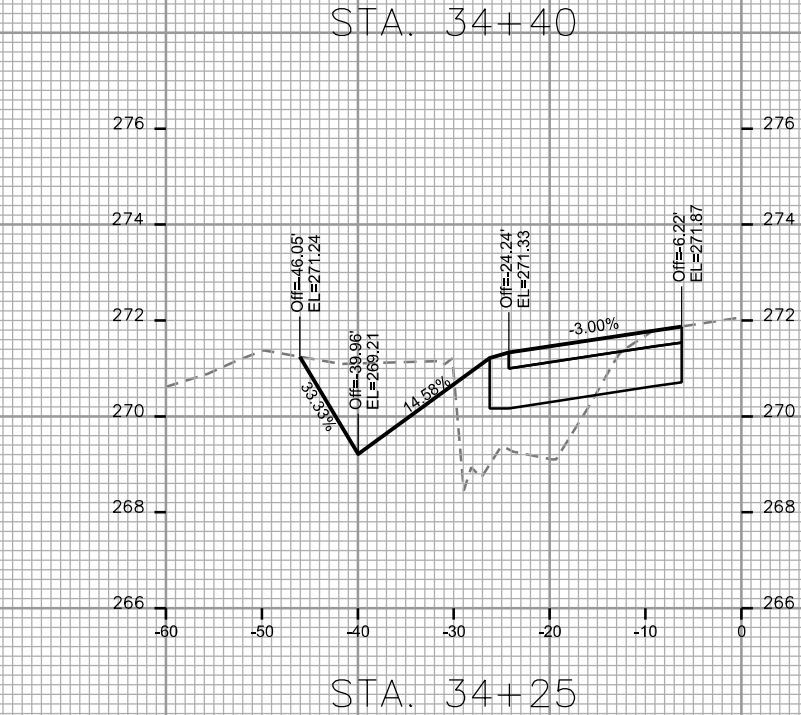
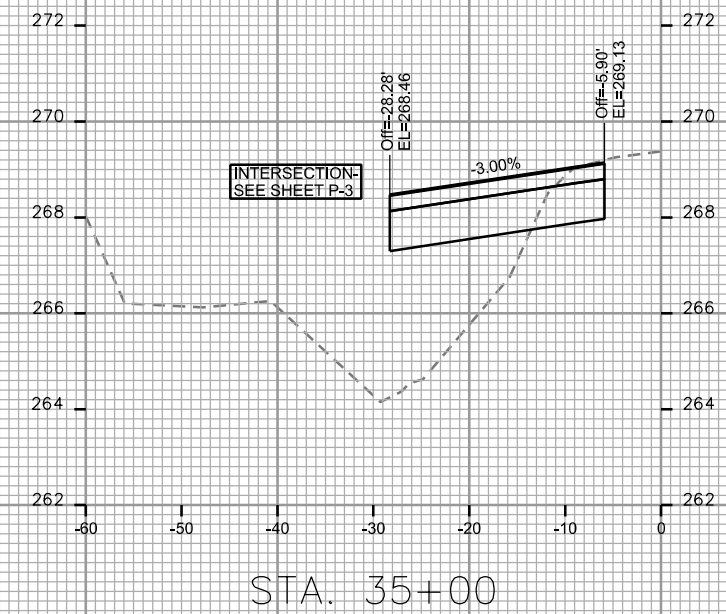
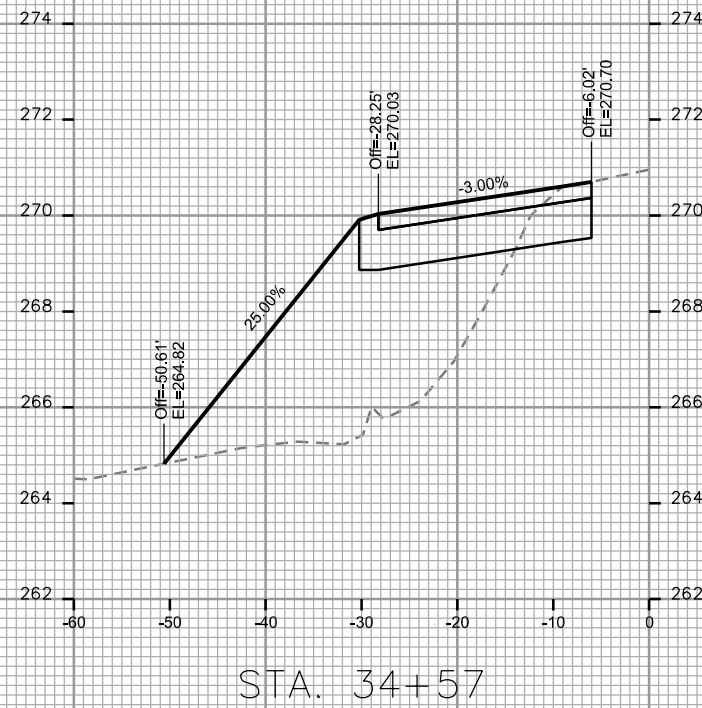
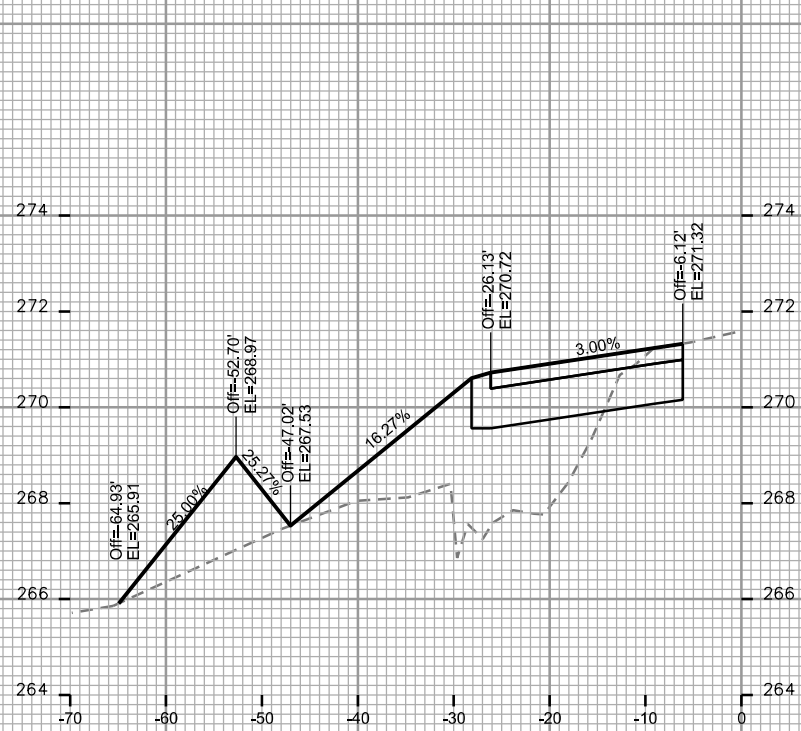
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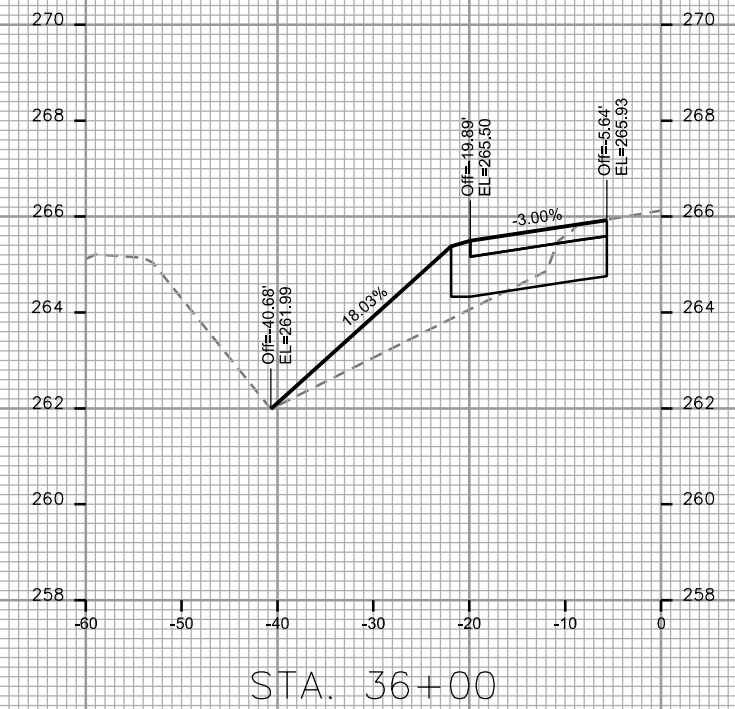
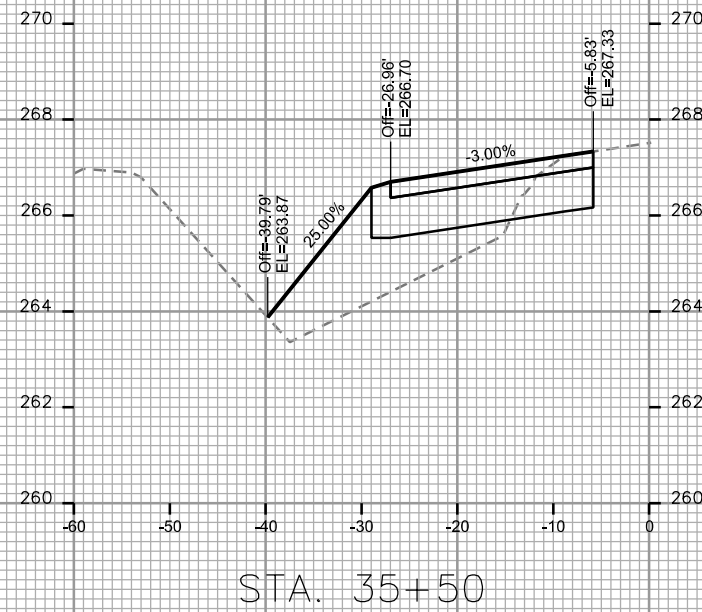
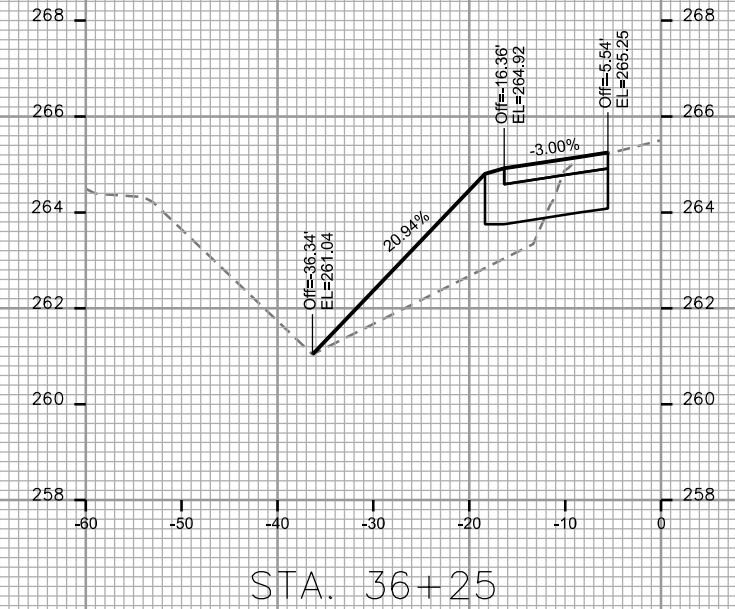
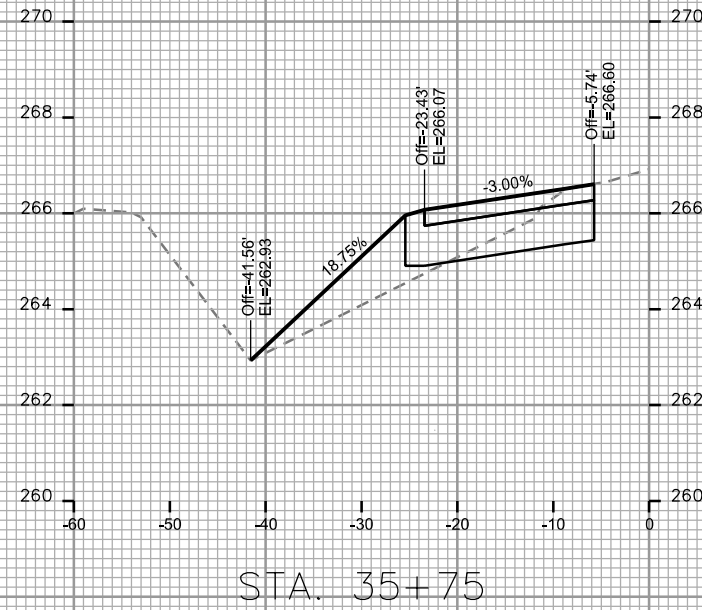
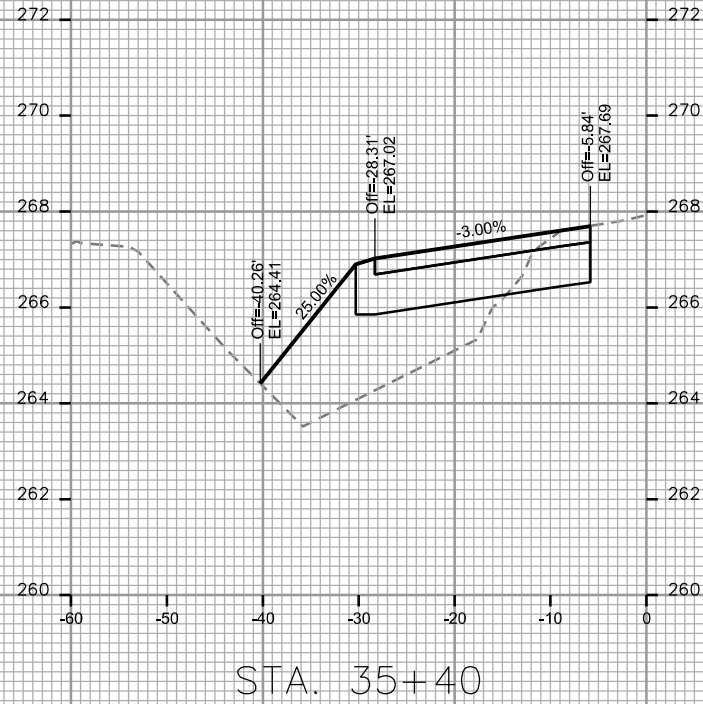
STAGE 2: OLD SAUK ROAD CITY OF MADISON

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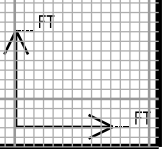
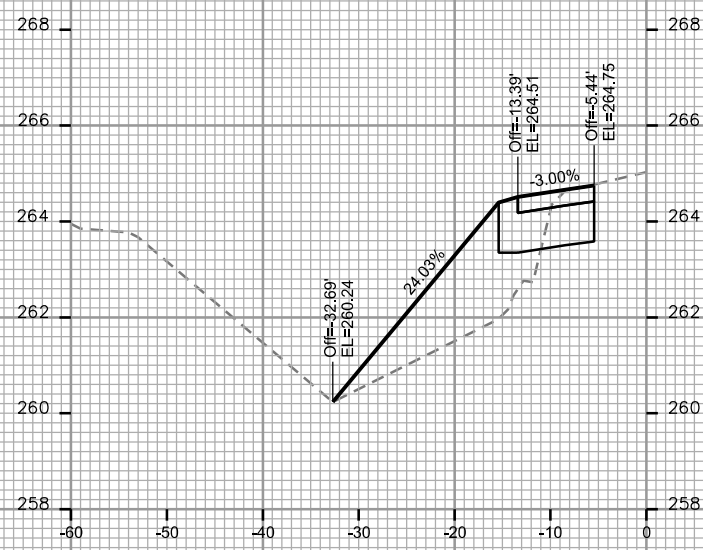
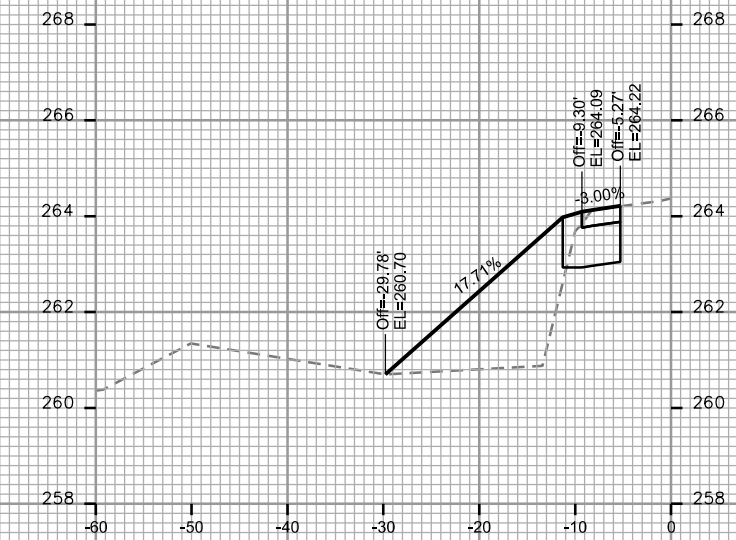
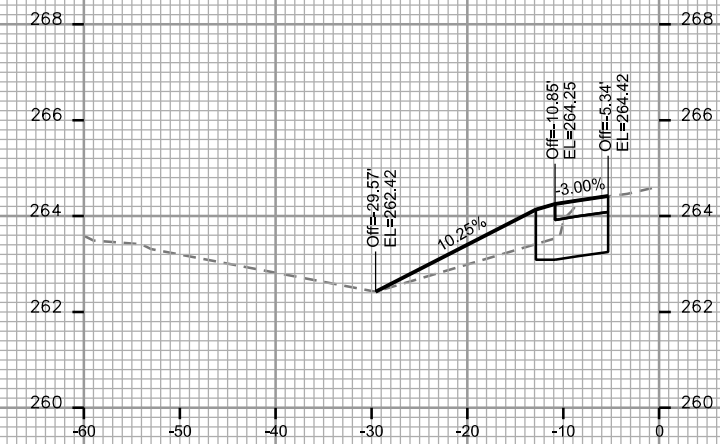
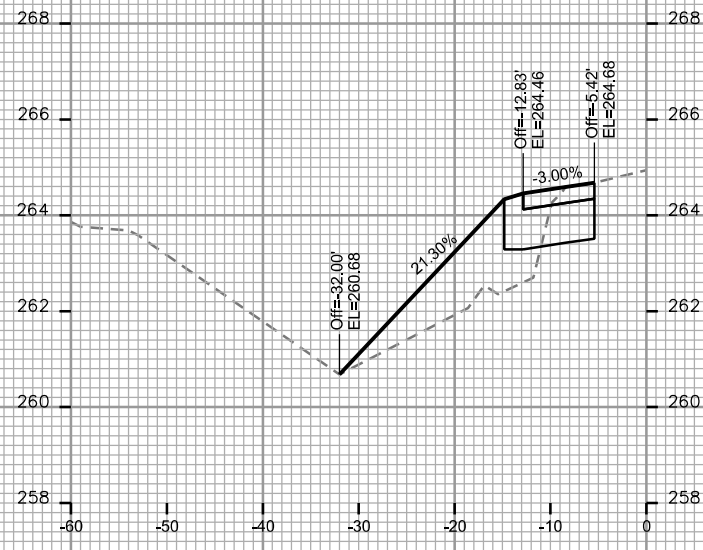
STAGE 2: OLD SAUK ROAD CITY OF MADISON

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CROSS SECTIONS

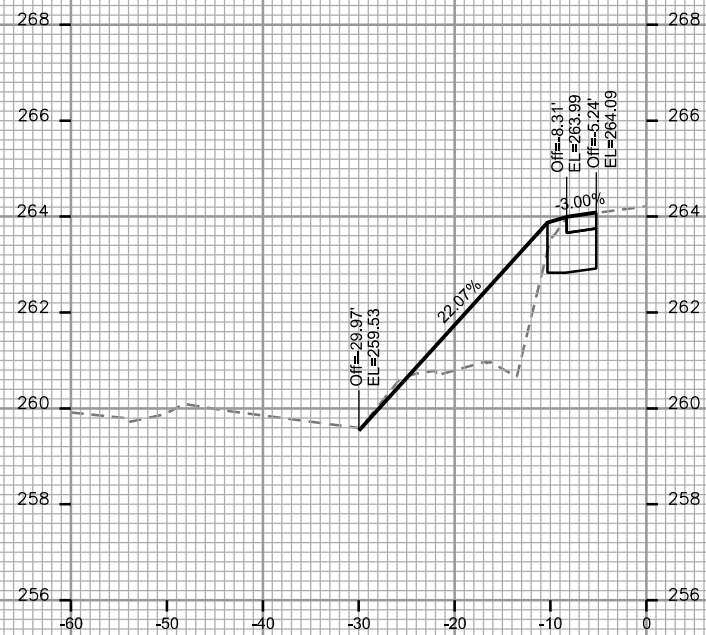
STAGE 2: OLD SAUK ROAD CITY OF MADISON

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REV. DATE: _____

ORIGINATOR: CITY OF MADISON - STREETS DIVISION



STA. 36+82

