

Madison, Wisconsin

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REV 7-21-14 JMW

SHEET NO.	TITLE
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CITY OF MADISON

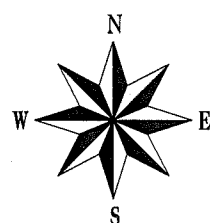
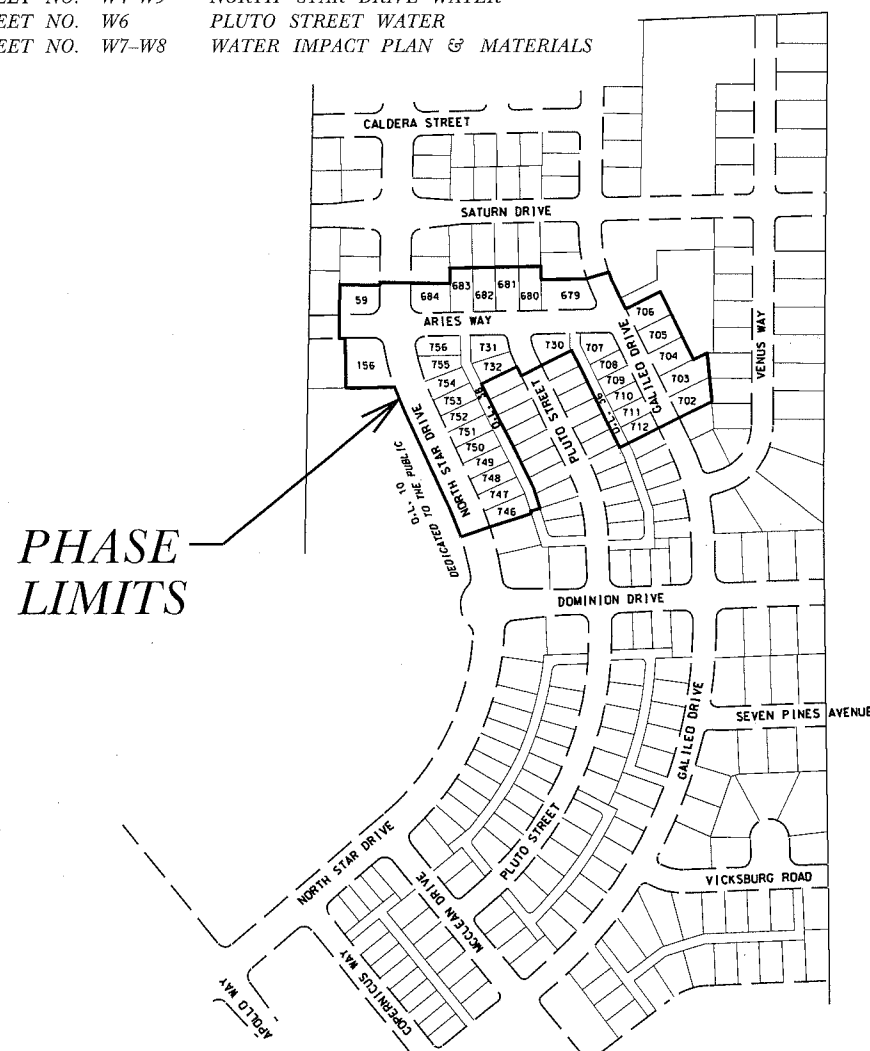
JOHN FAHRNEY/BRYAN/MANNING/ERIC HOEL REPORT THAT DKA APPARENTLY ERRED IN STAKING OUT AND CONSTRUCTING ARIES WAY. THE ROADWAY AND UTILITIES ARE SHIFTED TO THE SOUTH. FAHRNEY WILL REQUEST AS-BUILT SURVEY FROM DKA FOR CITY ENGR TO REVIEW TO DETERMINE RESOLUTION.

PLAN OF PROPOSED IMPROVEMENT GRANDVIEW COMMONS

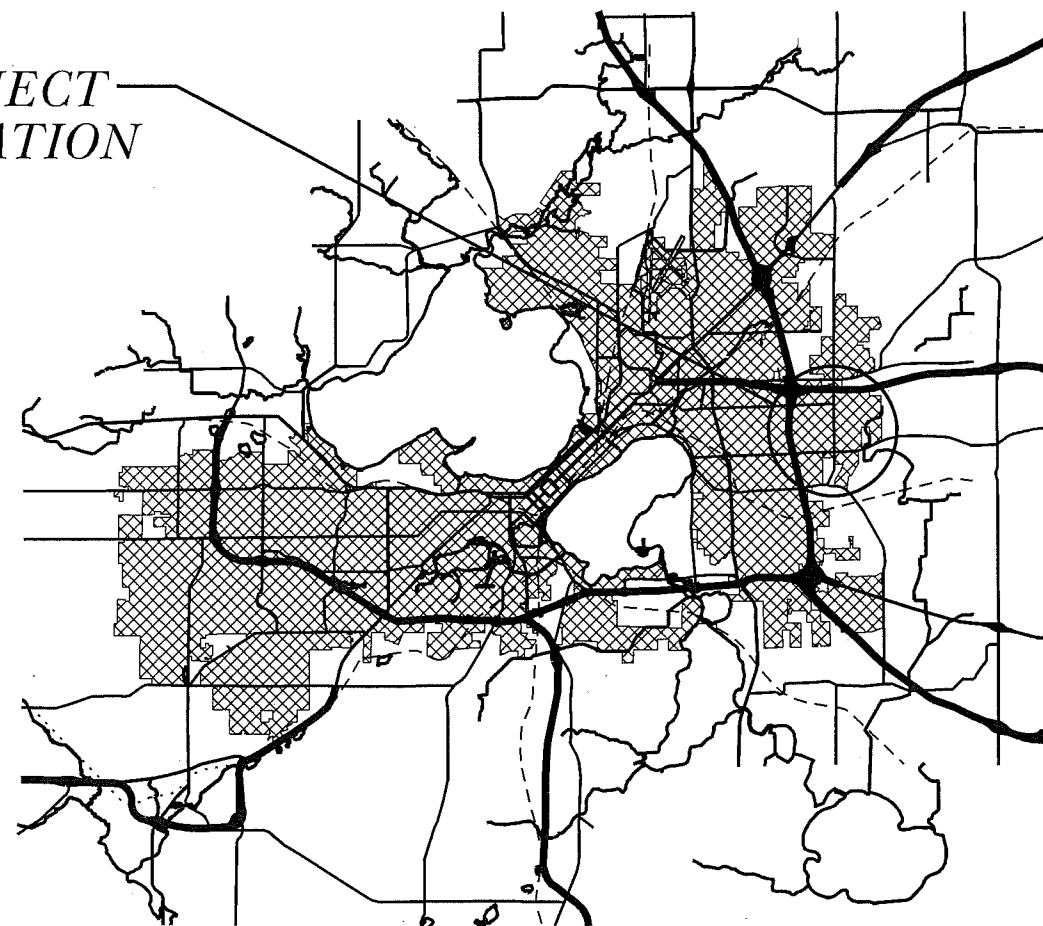
PHASE 20

ARIES WAY, GALILEO DRIVE, NORTH STAR DRIVE,
PLUTO STREET, O.L. 36 ALLEY, AND O.L. 38 ALLEY

CITY PROJECT NO. 53B2381
CONTRACT NO. 2381



PROJECT LOCATION



PUBLIC IMPROVEMENT PROJECT APPROVED
APRIL 29, 2014
BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

[Signature] 6/11/14
City Engineer Date

STREET DESIGNED BY

JAMES M. WOLFE
E-42725
MADISON, WI
5-30-14

STORM SEWER DESIGNED BY

GREGOR PRIES
E-33656
MADISON, WI
6/11/14

SANITARY SEWER DESIGNED BY

MARK D. MODERS
E-33979
MADISON, WI
6/10/14

WATER DISTRIBUTION DESIGNED BY

PETER E. HOLMGREN
E-42156
MADISON, WI
6/2/2014

THE LOCATION AND INFORMATION FOR PROPOSED NEW TREES, IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS ARE APPROXIMATE AND ARE SHOWN FOR REFERENCE ONLY. THE LOCATIONS, SPECIFICATIONS AND PLANTING METHODS OF ALL PROPOSED NEW OR REPLACEMENT TREES IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE APPROVED BY THE CITY FORESTER PRIOR TO INSTALLATION.

NO TREES IN THE RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE TRIMMED, PRUNED, REMOVED OR ADVERSELY AFFECTED IN ANY WAY UNTIL THE DEVELOPER HAS RECEIVED WRITTEN PERMISSION FROM THE CITY ENGINEER OR CITY FORESTER. SAID WRITTEN PERMISSION SHALL INCLUDE LANGUAGE INDICATING THAT SECTION 10.101 OF THE MADISON GENERAL ORDINANCES AND ADMINISTRATIVE PROCEDURE MEMORANDUM NO. 6-2, REFERING TO NOTIFICATION OF PROPERTY OCCUPANTS AND/OR OWNERS, HAS BEEN COMPLIED WITH.

ALL PAVEMENT IN THE NORTH STAR DR RIGHT-OF-WAY SHALL BE TYPE "B" AND ALL OTHER PAVEMENT SHALL BE TYPE "A" PER STANDARD DETAIL DRAWING 4.02.

ALL ISLANDS SHALL HAVE TYPE "E" C & G PER S.D.D. 3.08 AND MOUNTABLE NOSES PER S.D.D. 3.13. ALL OTHER C & G SHALL BE TYPE "A" PER S.D.D. 3.06.

ISLAND INTERIORS SHALL BE OF SOD WHERE THE BACK TO BACK OF CURB DIMENSION IS GREATER THAN 6'.

UNDERDRAINS SHALL BE INSTALLED, PER STANDARD DETAIL DRAWING 4.05, ON PAGE 2 OF THESE PLANS, FOR 75' ON EACH SIDE OF THE LOW POINT, OR TO THE NEAREST CURB HIGH POINT. ALL UNDERDRAIN SHALL BE WRAPPED.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADES OF 0.5% TOWARD STORM SEWER INLETS.

PAVEMENT CROSS SLOPES SHALL BE 2%, TERRACES SHALL SLOPE AT A 4% GRADE TOWARD THE GUTTER, SHOULDERS SHALL HAVE A 4% CROSS SLOPE AND CROSS SLOPES FORMING DITCHES SHALL NOT EXCEED 4.00:1 UNLESS OTHERWISE INDICATED.

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL BE 2%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.03. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. A 2 FOOT WIDE CONCRETE FLUME SHALL BE INSTALLED BETWEEN THE SIDEWALK AND THE BACK OF CURB AT SIDEWALK LOW POINTS WHICH CANNOT OTHERWISE BE DRAINED DIRECTLY TO THE GUTTER. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4.00:1. THE DESIGN OF SIDEWALK ELEVATIONS AND GRADES IS NOT INCLUDED IN THIS PLAN. ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENT FROM THE CITY ENGINEER PRIOR TO STAKING THIS PROJECT.

CURB STATION AND OFFSETS SHALL BE TO THE FACE OF CURB UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE TOP OF CURB (OR EXTENDED TOP OF CURB FOR DRIVEWAYS OR RAMPS) UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

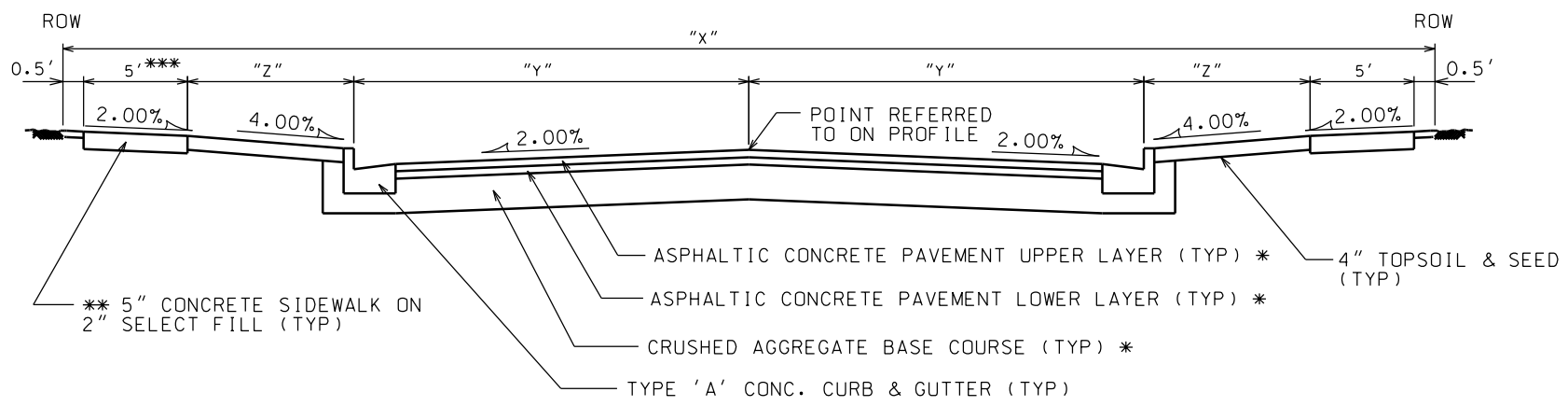
CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO SDD 3.05.

PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

TYPICAL SECTION - PLUTO STREET



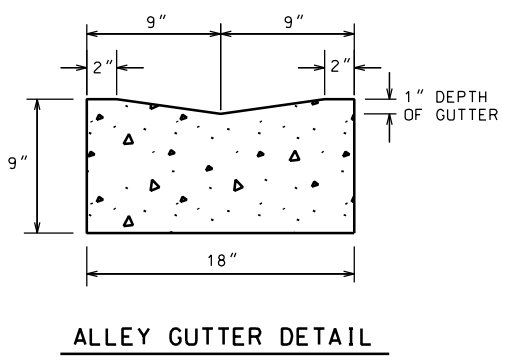
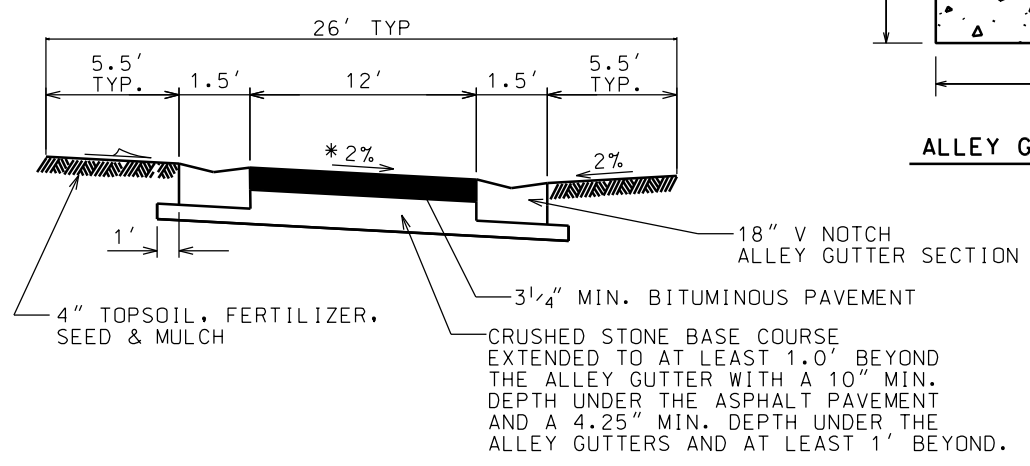
NOTES:

- * A PAVEMENT DESIGN IN ACCORDANCE WITH STANDARD DETAIL DRAWING 4.06 IS REQUIRED ON ALL PROJECTS
- **3" OF CRUSHED AGGREGATE BASE COURSE GRADATION 3 SHALL BE PLACED UNDER THE SIDEWALK WHERE THE CENTERLINE GRADE OF THE STREET EXCEEDS 5%
- *** SIDEWALK TO BE 6' WIDE ALONG NORTH STAR DRIVE ALONG FRONTAGE FOR D.L.10 (STA 41+07 TO STA 45+06)

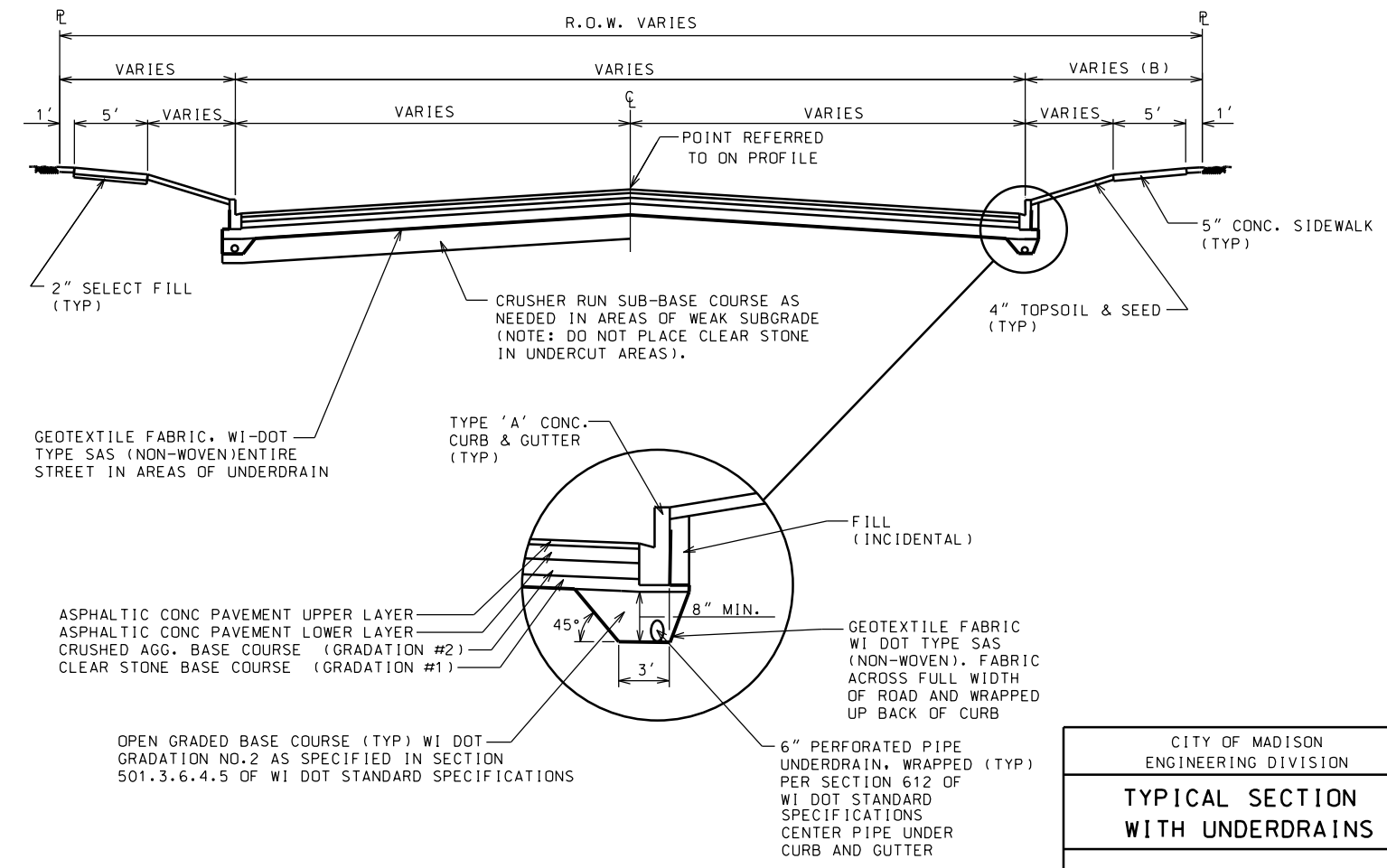
* CITY OF MADISON MINIMUM PAVEMENT DESIGN

TYPE	CRUSHED AGGREGATE BASE COURSE		ASPHALTIC CONCRETE PAVEMENT			
	LOWER LAYER GRADATION 1	UPPER LAYER GRADATION 2	LOWER LAYER TYPE	LOWER LAYER THICKNESS	UPPER LAYER TYPE	UPPER LAYER THICKNESS
A	6"	4"	E-0.3	1.75"	E-0.3	1.75"
B	6"	4"	E-1	2.5"	E-1	1.75"
C	6"	4"	E-3	3.50"	E-3	1.75"

TYPICAL ALLEY SECTION



UNDERDRAIN INSTALLATION



* SEE ARROWS ON PLAN PAVEMENT FOR DIRECTION OF CROSS SLOPE.

CURBS SHALL HAVE A MINIMUM 0.5% LONGITUDINAL GRADE TOWARD INLETS.

ALL STORM WATER SHALL BE COLLECTED BY STORM SEWER INLETS PRIOR TO FLOWING OVER PRIVATE PROPERTY OR SIDEWALKS.

HORIZONTAL ALIGNMENT NOTE
EDGE OF PAVEMENT RADIi SHALL NOT BE LESS THAN 25 FEET

THE EDGE OF ALLEY PAVEMENT SHALL NOT BE CLOSER THAN 7.0' TO THE PROPERTY LINE AT ANY POINT.

CITY OF MADISON
ENGINEERING DIVISION

**TYPICAL SECTION
WITH UNDERDRAINS**

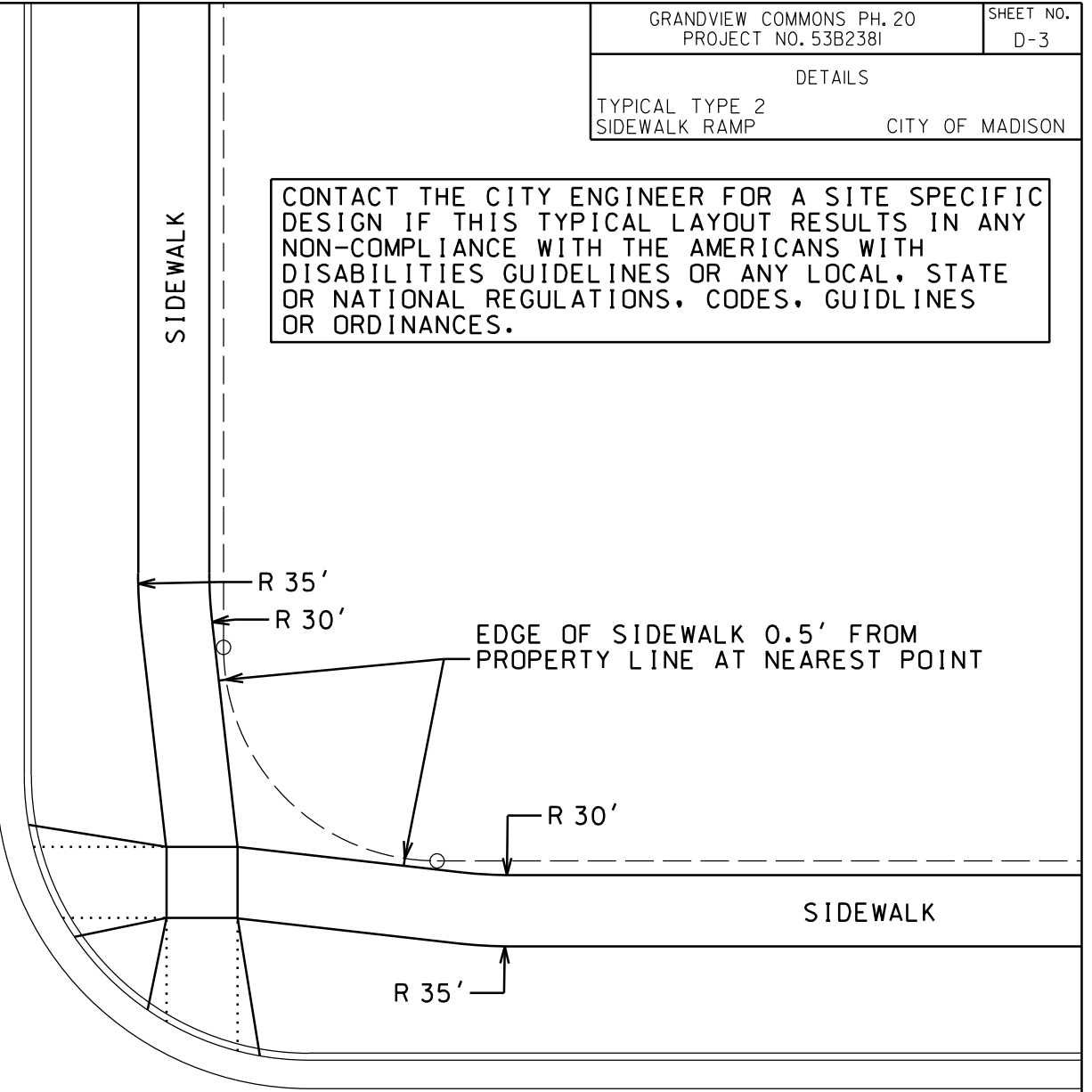
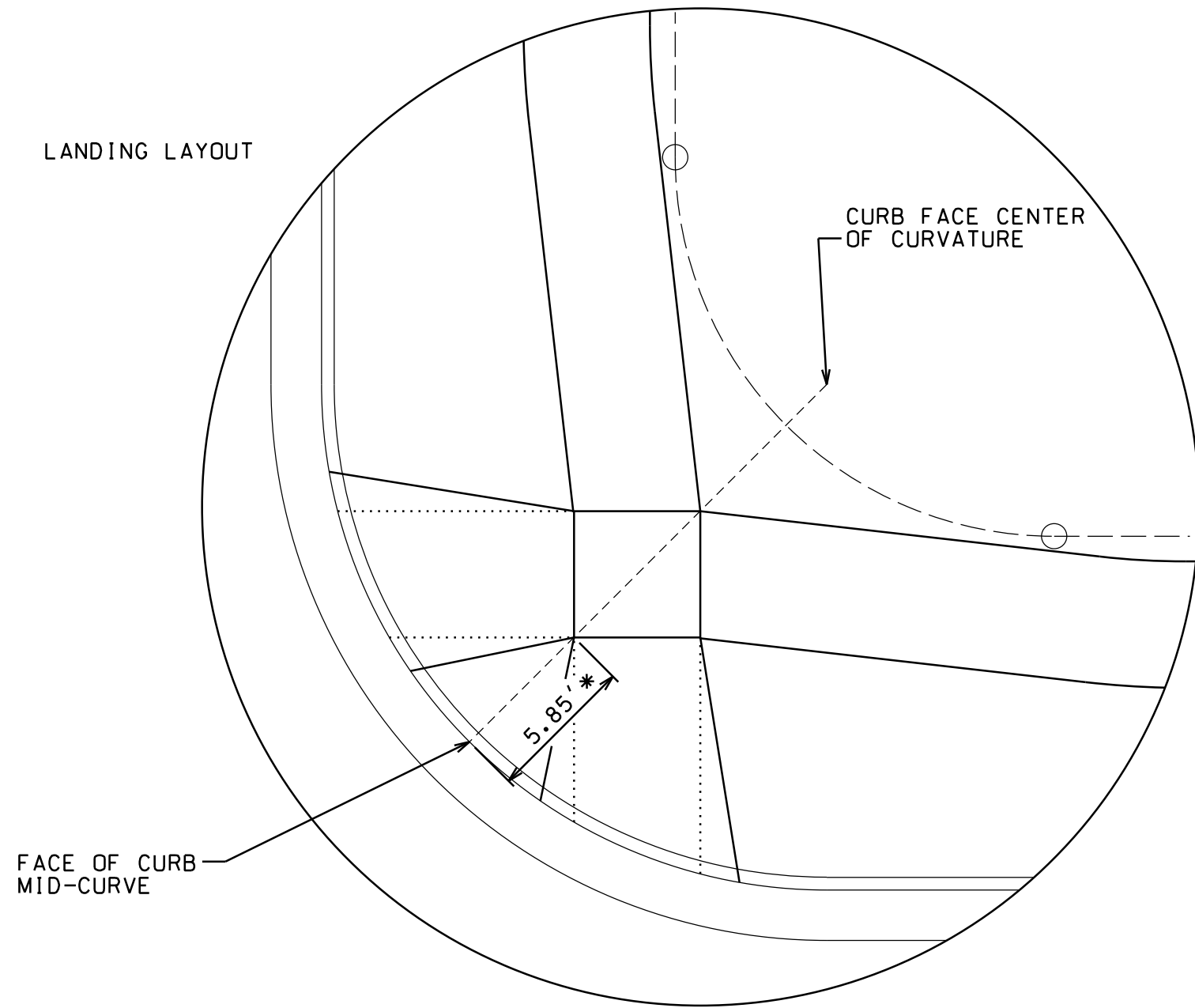
STANDARD DETAIL DRAWING 4.05

PLOT SCALE: _____
PLOT NAME: _____
REV. DATE: _____
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

ANY SIDEWALK ELEVATIONS INDICATED ON THE PLAN NEAR THE RAMPS ARE APPROXIMATE AND NOT TO BE USED FOR CONSTRUCTION. ACTUAL SIDEWALK AND RAMP ELEVATIONS SHALL BE DETERMINED BASED ON THE AS BUILT ELEVATIONS OF THE CURB AND GUTTER.

CONTACT THE CITY ENGINEER FOR A SITE SPECIFIC DESIGN IF THIS TYPICAL LAYOUT RESULTS IN ANY NON-COMPLIANCE WITH THE AMERICANS WITH DISABILITIES GUIDELINES OR ANY LOCAL, STATE OR NATIONAL REGULATIONS, CODES, GUIDLINES OR ORDINANCES.

LANDING LAYOUT



* THE NEAR CORNER OF THE LANDING SHALL BE 5.85' FROM THE FACE OF CURB, UNLESS SPECIFIED OTHERWISE ON THE PLAN. BOTH OPPOSITE CORNERS OF THE LANDING SHALL BE LOCATED ON THE FACE OF CURB RADIUS LINE WHICH PASSES THROUGH THE MIDPOINT OF THE CURB CURVATURE.

LONGITUDINAL RAMP GRADES SHALL NOT EXCEED 1"/12" NOR BE LESS THAN 2.0% AT ANY POINT. THIS NOTE SHALL TAKE PRECEDENCE OVER ANY ELEVATION OR GRADE INDICATED ON THE PLAN.

SIDEWALK LONGITUDINAL GRADES AT ALL LOCATIONS OTHER THAN RAMPS AND WHERE STREET GRADES ARE MORE THAN 5.0% SHALL NOT EXCEED 5.0%. WHERE STREET GRADES EXCEED 5.0%, THE SIDEWALK LONGITUDINAL GRADE SHALL NOT EXCEED THE STREET GRADE.

THE RAMPS SHALL ALIGN
WITH THE CROSSINGS.

SIDEWALK

SIDEWALK

SIDEWALK

SIDEWALK

SIDEWALK

SIDEWALK

SIDEWALK

SIDEWALK

CONTACT THE CITY ENGINEER FOR A SITE SPECIFIC
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OR ORDINANCES.

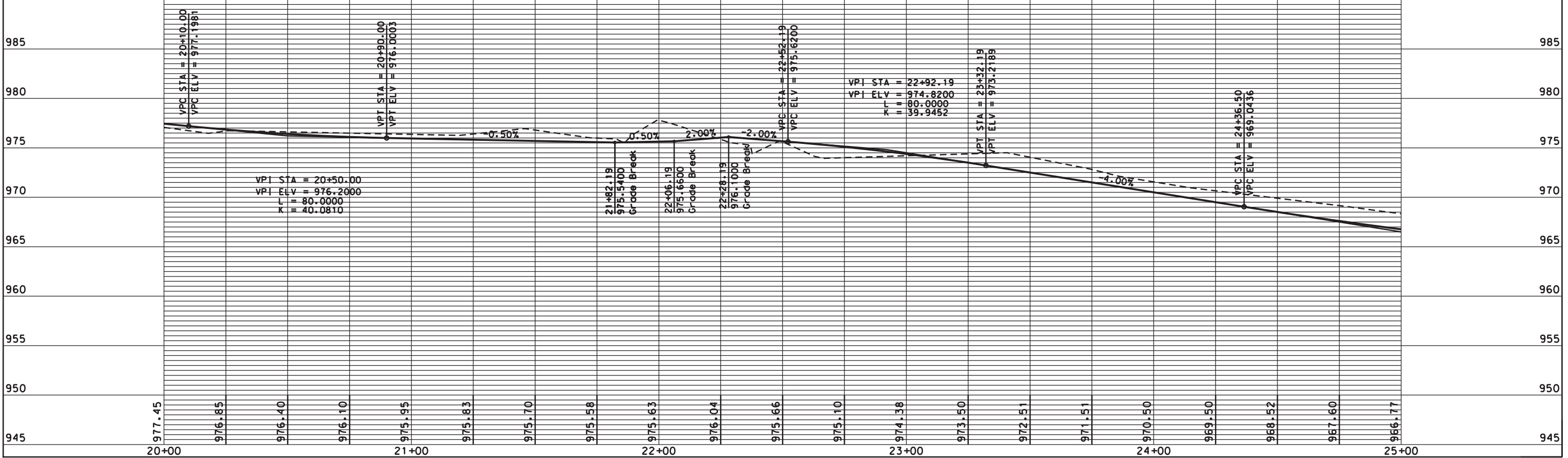
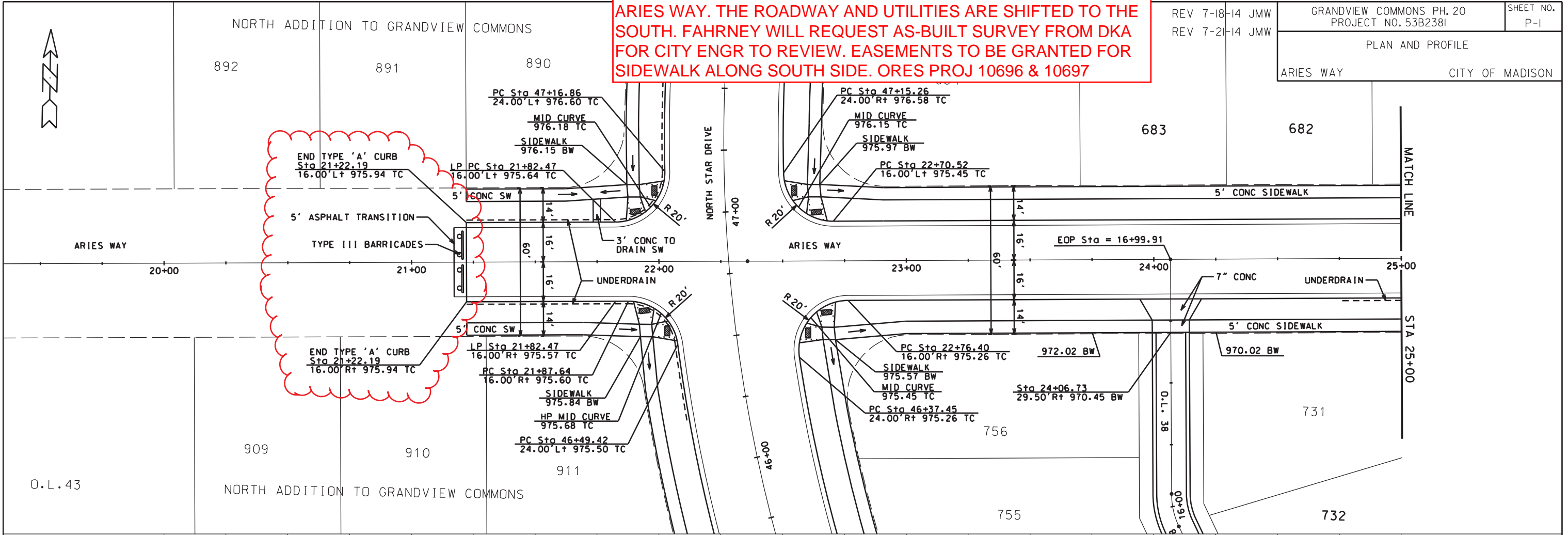
PLOT SCALE:

PLOT NAME:

REV. DATE:

JOHN FAHRNEY/BRYAN/ MANNING/ERIC HOEL REPORT THAT DKA APPARENTLY ERRED IN STAKING OUT AND CONSTRUCTING ARIES WAY. THE ROADWAY AND UTILITIES ARE SHIFTED TO THE SOUTH. FAHRNEY WILL REQUEST AS-BUILT SURVEY FROM DKA FOR CITY ENGR TO REVIEW. EASEMENTS TO BE GRANTED FOR SIDEWALK ALONG SOUTH SIDE. ORES PROJ 10696 & 10697

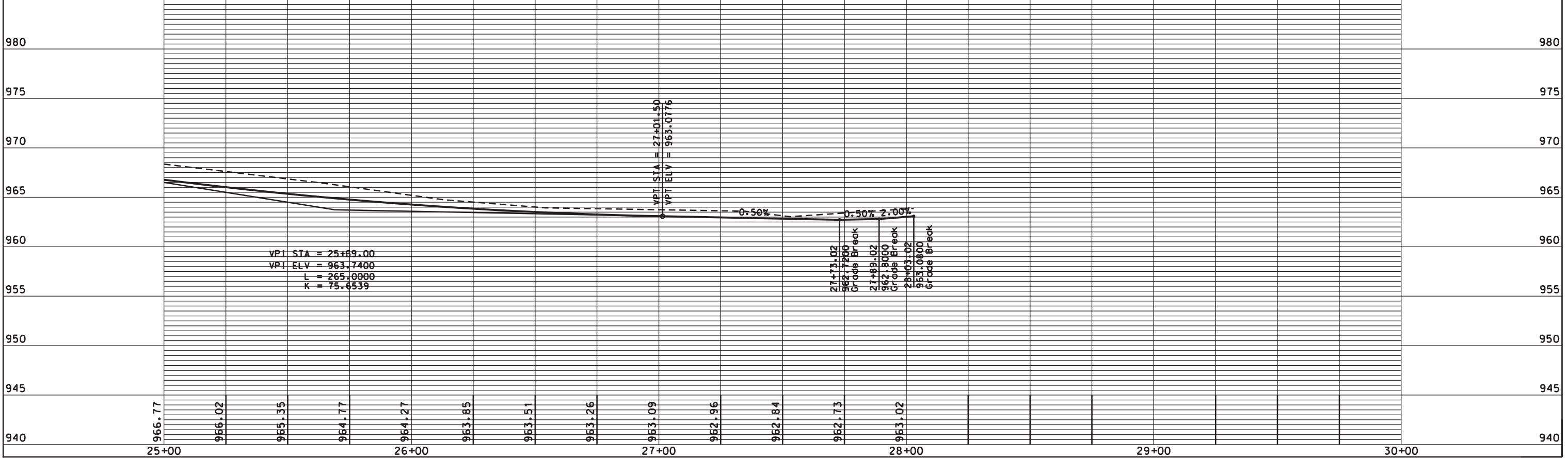
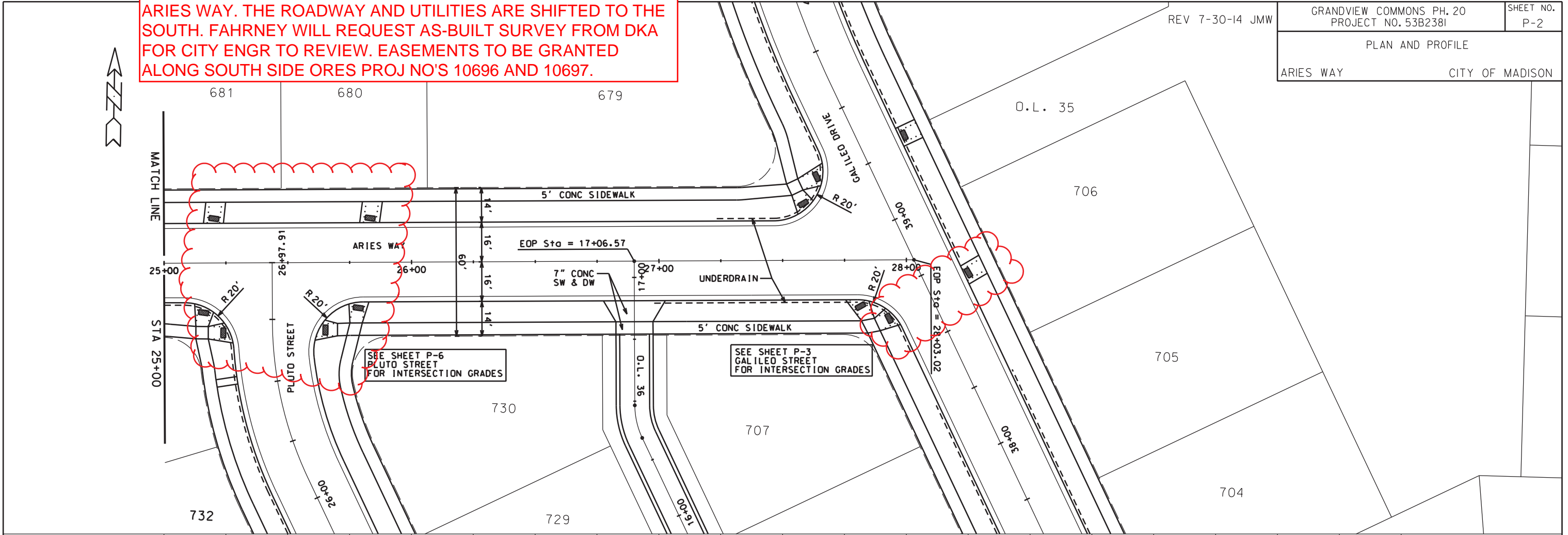
REV 7-18-14 JMW	GRANDVIEW COMMONS PH. 20	SHEET NO.
REV 7-21-14 JMW	PROJECT NO. 53B2381	P-1
PLAN AND PROFILE		
ARIES WAY		CITY OF MADISON



ORIGINATOR: CITY OF MADISON STREETS DIVISION

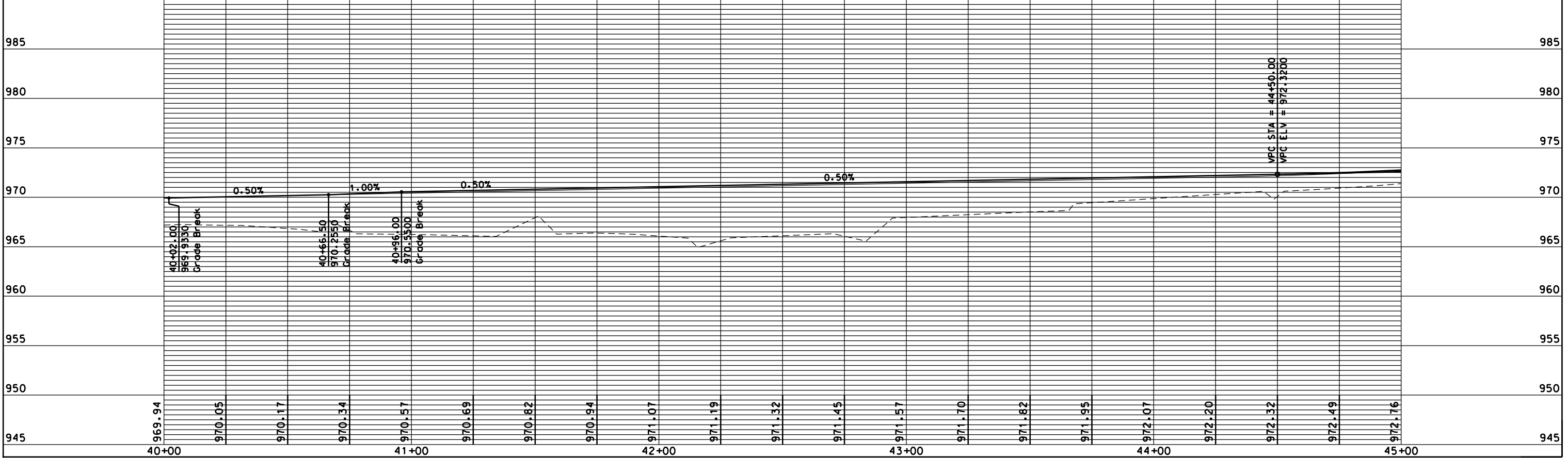
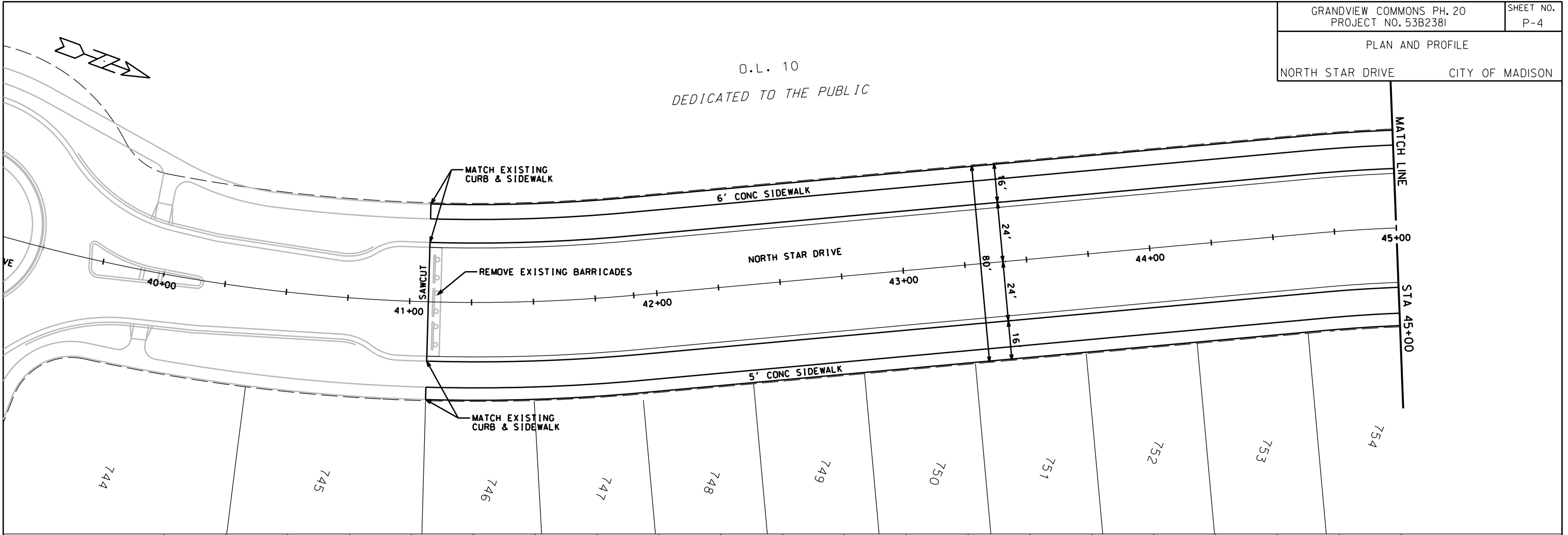
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REV 7-30-14 JMW



ORIGINATOR: CITY OF MADISON STREETS DIVISION

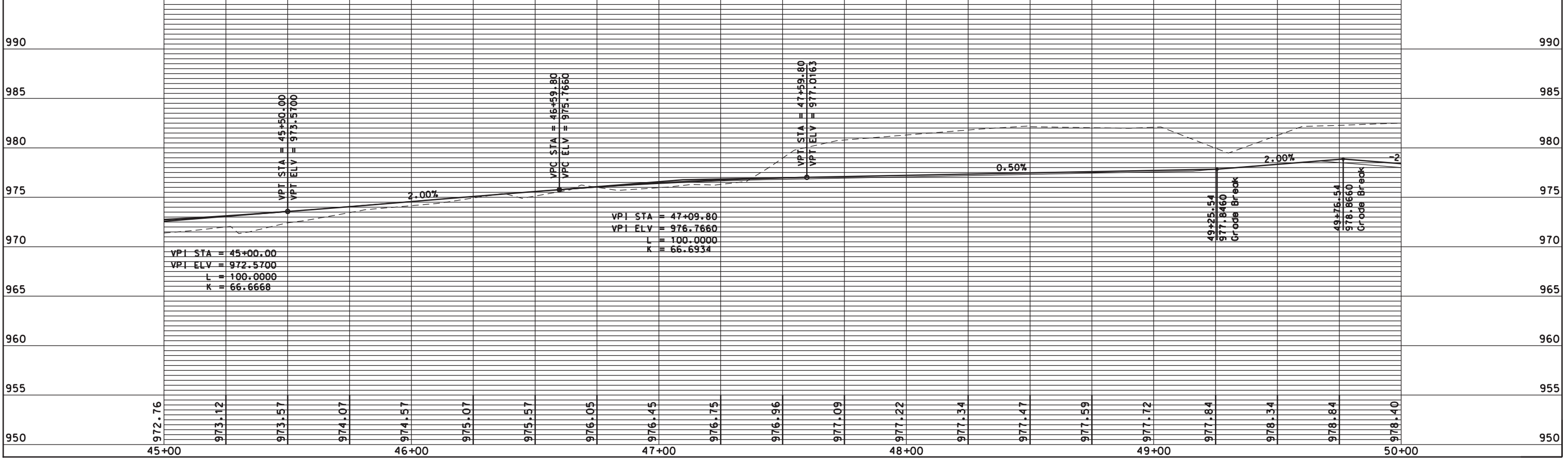
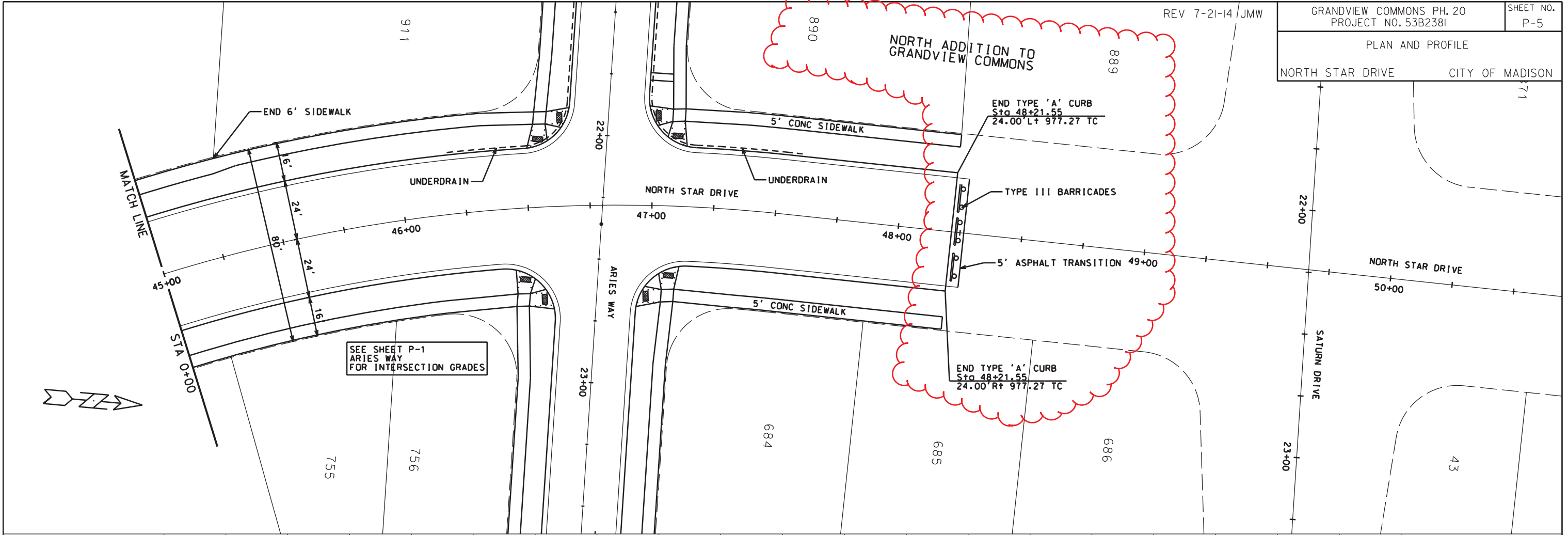
O.L. 10
DEDICATED TO THE PUBLIC



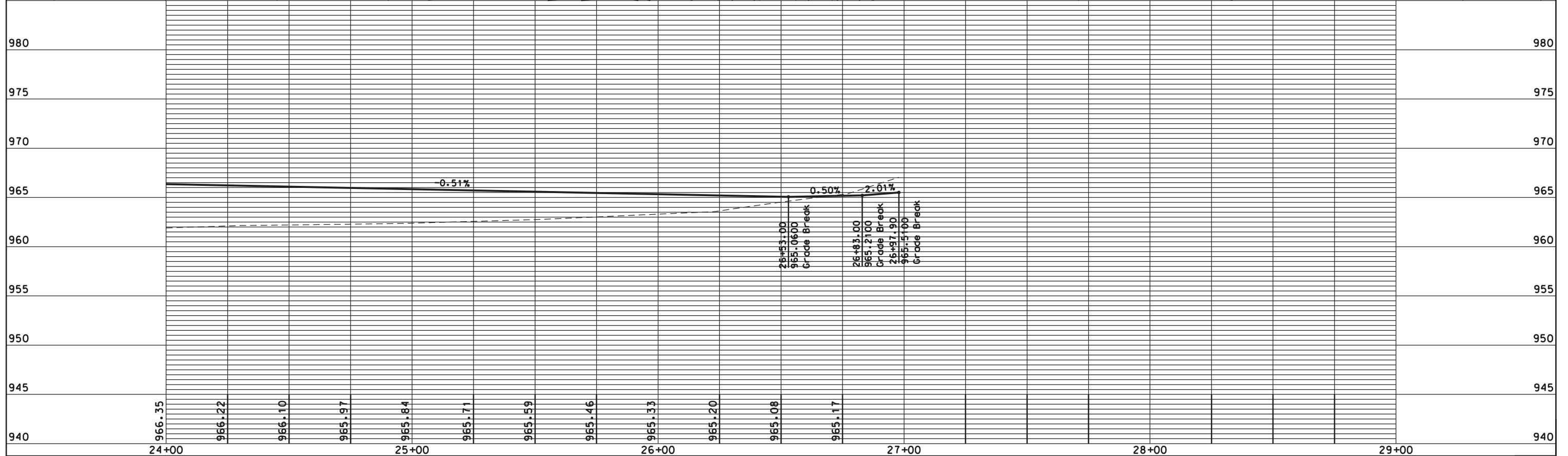
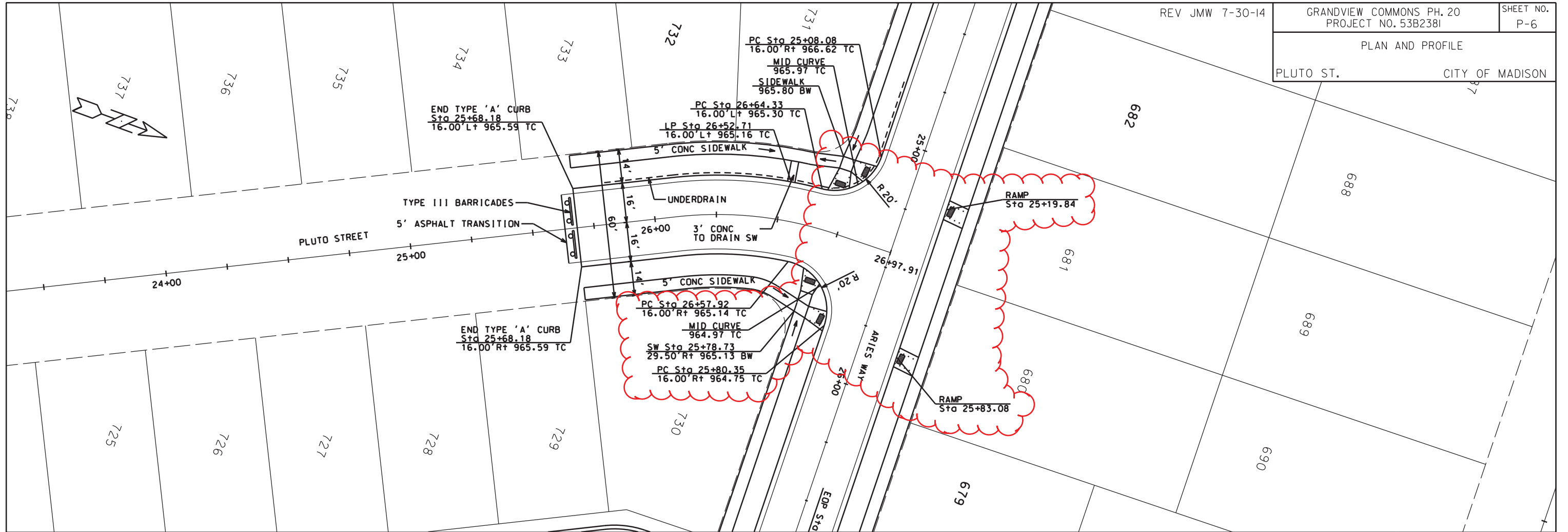
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REV. DATE: _____

ORIGINATOR: CITY OF MADISON STREETS DIVISION

REV 7-21-14/JMW

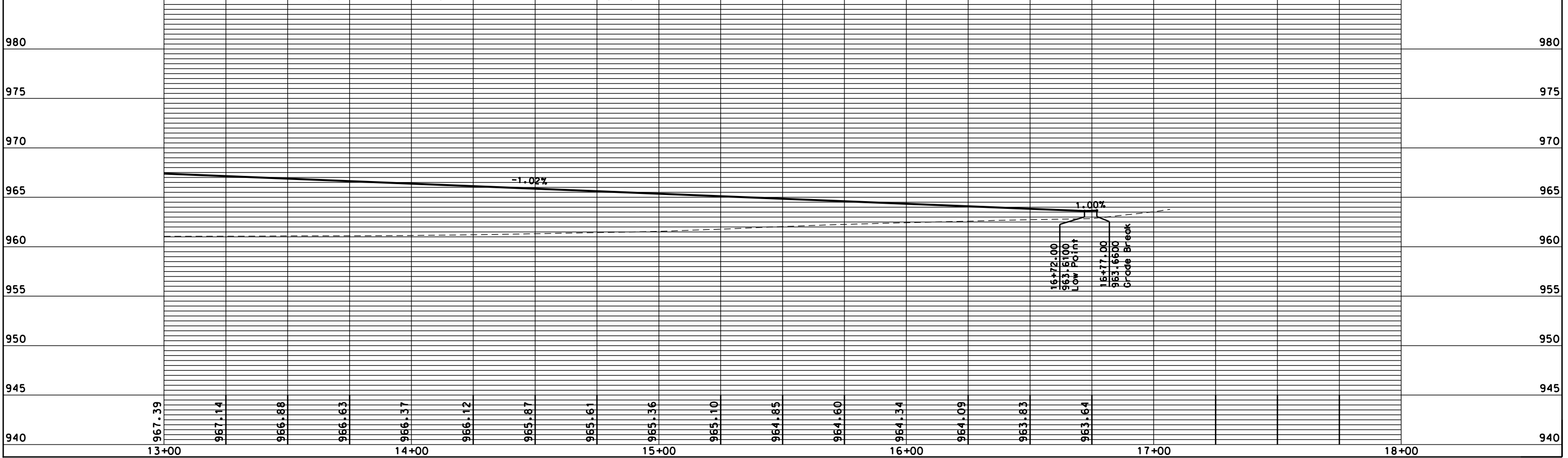
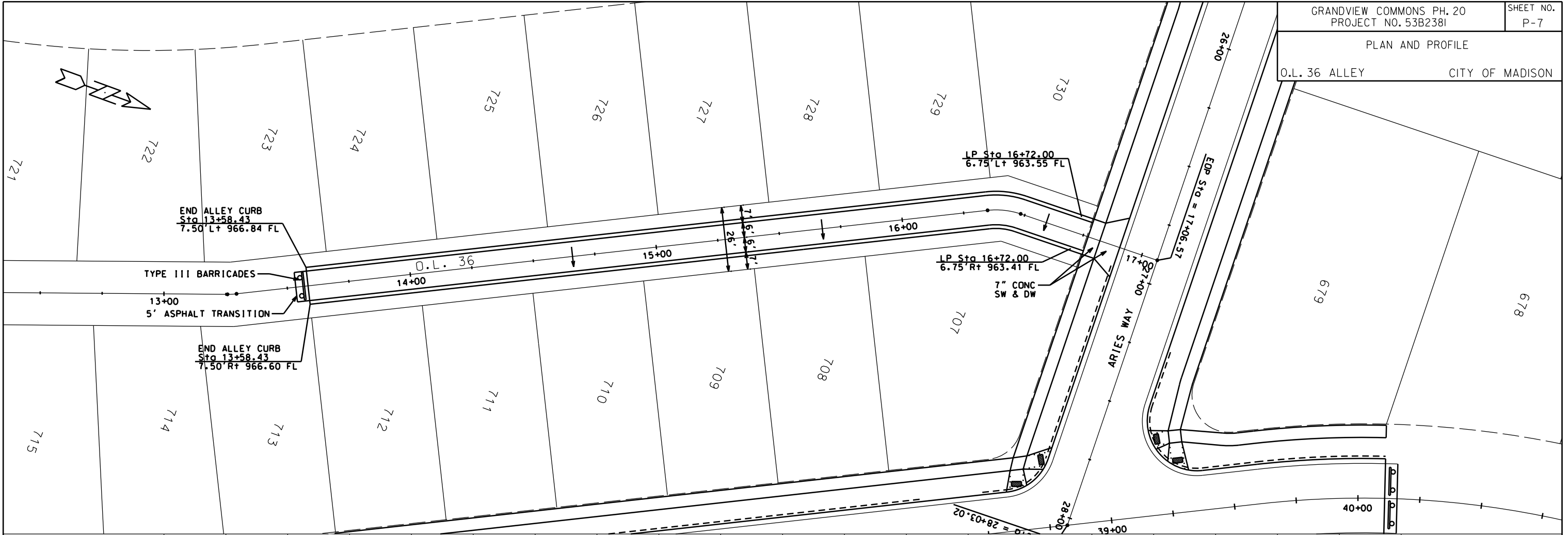


ORIGINATOR: CITY OF MADISON STREETS DIVISION



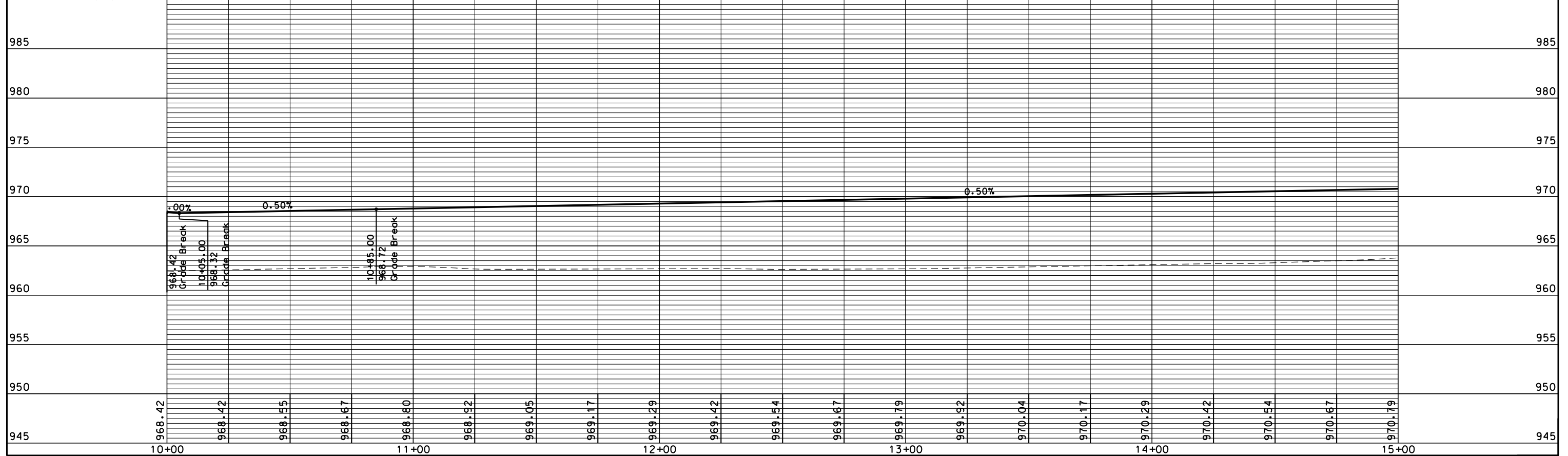
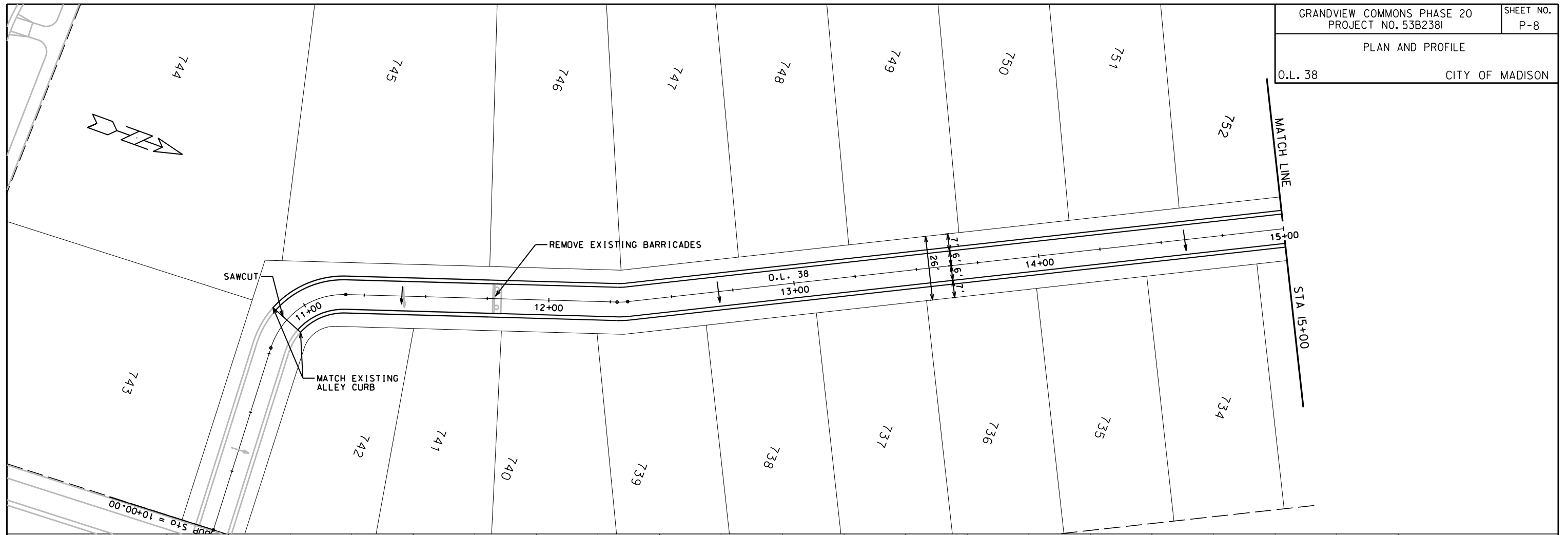
ORIGINATOR: CITY OF MADISON STREETS DIVISION

PLAN AND PROFILE
O.L. 36 ALLEY CITY OF MADISON



PLOT SCALE: _____
PLOT NAME: _____
REV. DATE: _____

ORIGINATOR: CITY OF MADISON STREETS DIVISION

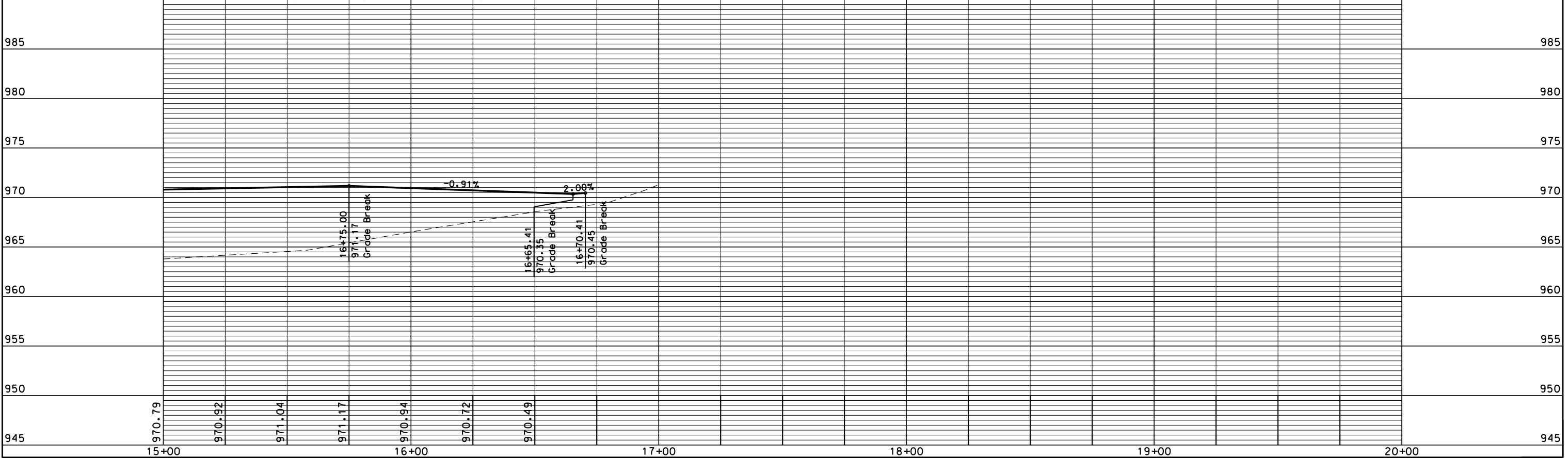
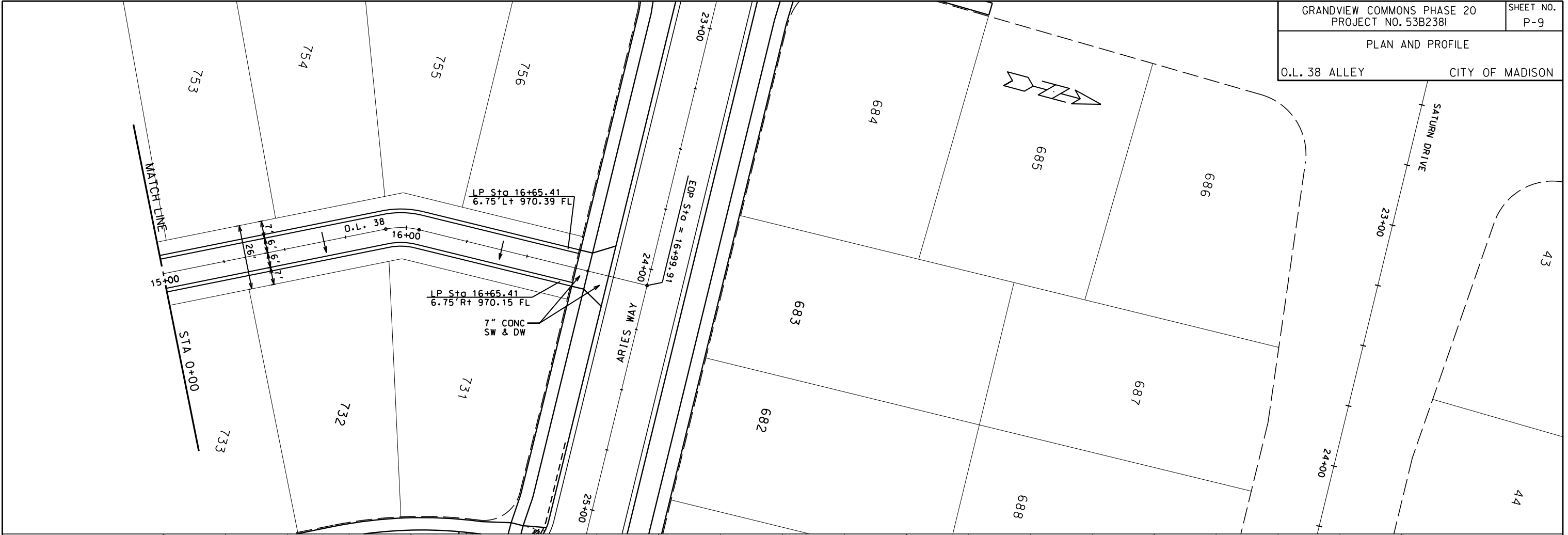


ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLOT SCALE:

PLOT NAME:

REV. DATE:



PLOT SCALE: _____

PLOT NAME: _____

REV. DATE: _____

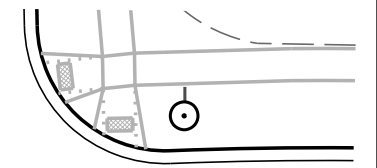
ORIGINATOR: CITY_OF_MADISON\STREETS_DIVISION

ELECTRICAL PLAN

N STARR DR, ARIES WY CITY OF MADISON



LEGEND
⊙ LB-3 BASE WITH 3" ELBOW



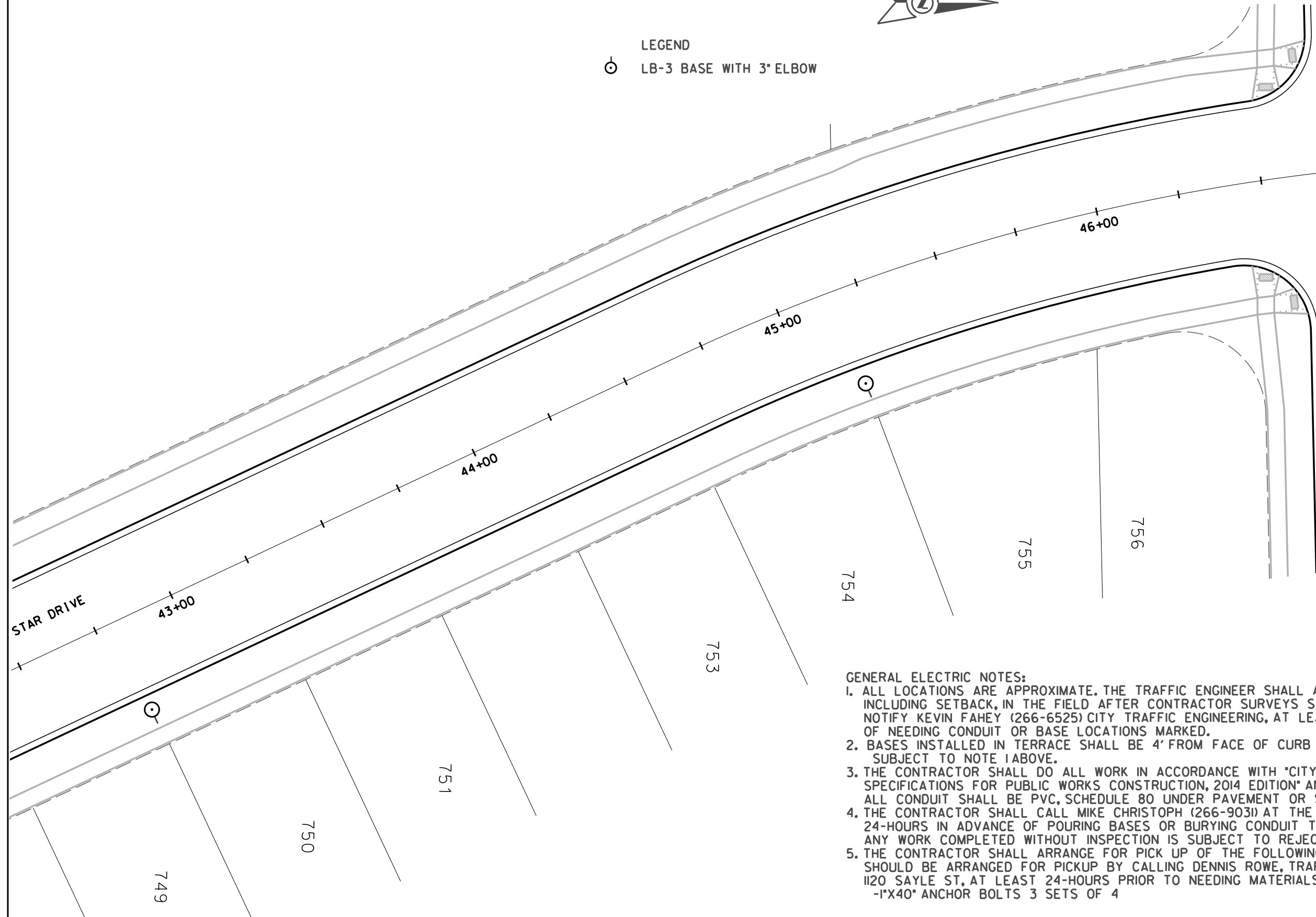
NORTH STAR DRIVE

47+00

ARIES WAY

22+00

23+00



GENERAL ELECTRIC NOTES:

1. ALL LOCATIONS ARE APPROXIMATE. THE TRAFFIC ENGINEER SHALL APPROVE FINAL LOCATIONS, INCLUDING SETBACK, IN THE FIELD AFTER CONTRACTOR SURVEYS STAKING. THE CONTRACTOR SHALL NOTIFY KEVIN FAHEY (266-6525) CITY TRAFFIC ENGINEERING, AT LEAST 24-HOURS IN ADVANCE OF NEEDING CONDUIT OR BASE LOCATIONS MARKED.
2. BASES INSTALLED IN TERRACE SHALL BE 4' FROM FACE OF CURB UNLESS OTHERWISE NOTED. SUBJECT TO NOTE 1 ABOVE.
3. THE CONTRACTOR SHALL DO ALL WORK IN ACCORDANCE WITH "CITY OF MADISON STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 2014 EDITION" AND ALL ADDENDUMS THERETO. ALL CONDUIT SHALL BE PVC, SCHEDULE 80 UNDER PAVEMENT OR SCHEDULE 40 OTHERWISE.
4. THE CONTRACTOR SHALL CALL MIKE CHRISTOPH (266-9031) AT THE TRAFFIC ENGINEERING SHOP AT LEAST 24-HOURS IN ADVANCE OF POURING BASES OR BURYING CONDUIT TO ARRANGE FOR INSPECTION. ANY WORK COMPLETED WITHOUT INSPECTION IS SUBJECT TO REJECTION.
5. THE CONTRACTOR SHALL ARRANGE FOR PICK UP OF THE FOLLOWING CITY FURNISHED MATERIALS, WHICH SHOULD BE ARRANGED FOR PICKUP BY CALLING DENNIS ROWE, TRAFFIC ENGINEERING SHOP, 266-9034 1120 SAYLE ST, AT LEAST 24-HOURS PRIOR TO NEEDING MATERIALS:
-1"X40" ANCHOR BOLTS 3 SETS OF 4

PLOT SCALE:

PLOT NAME:

REV. DATE:

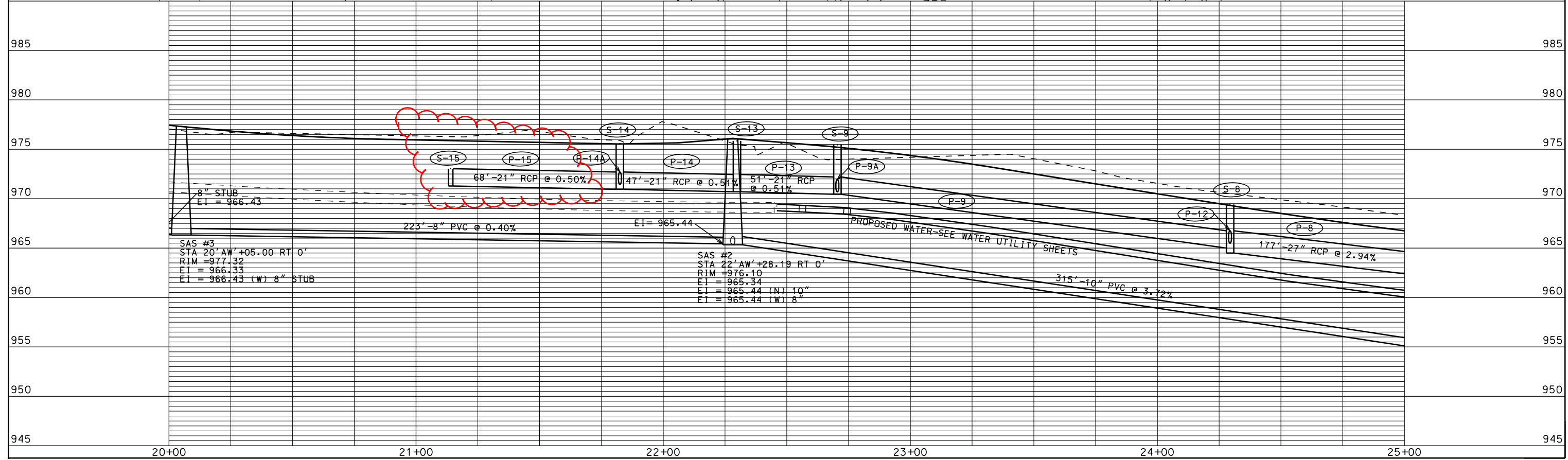
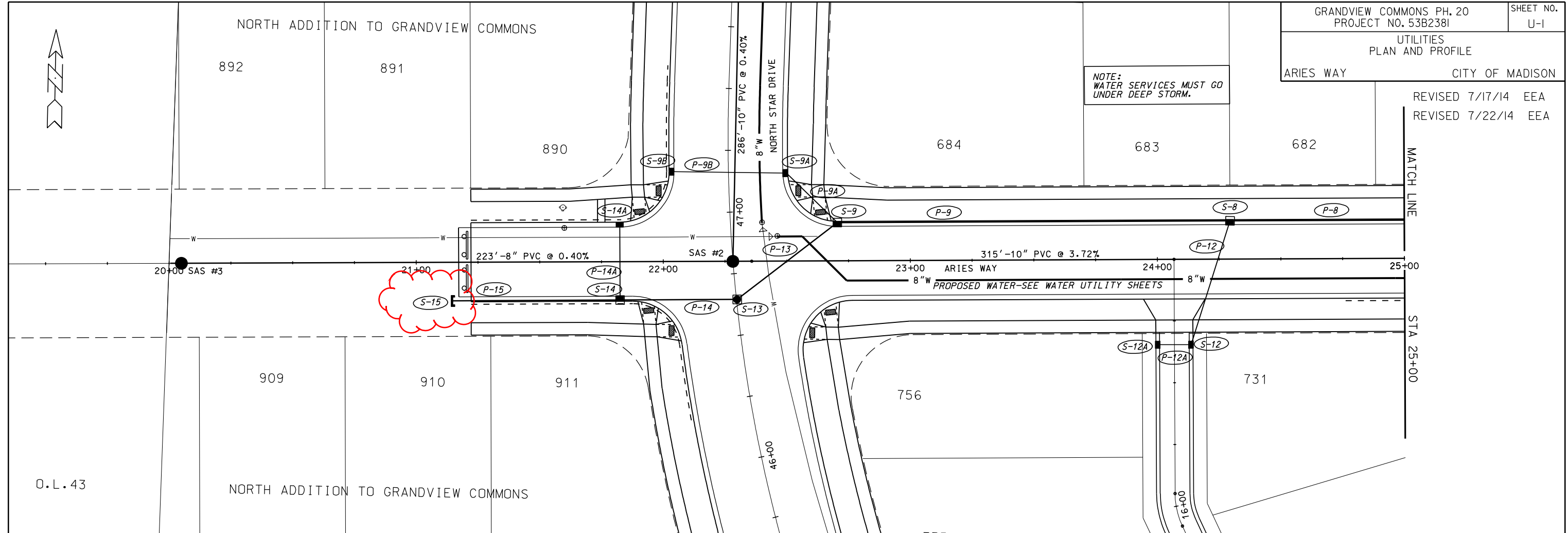
ORIGINATOR: CITY OF MADISON, TRAFFIC ENG. DIV.



PLOT SCALE:

PLOT NAME:

REV. DATE:

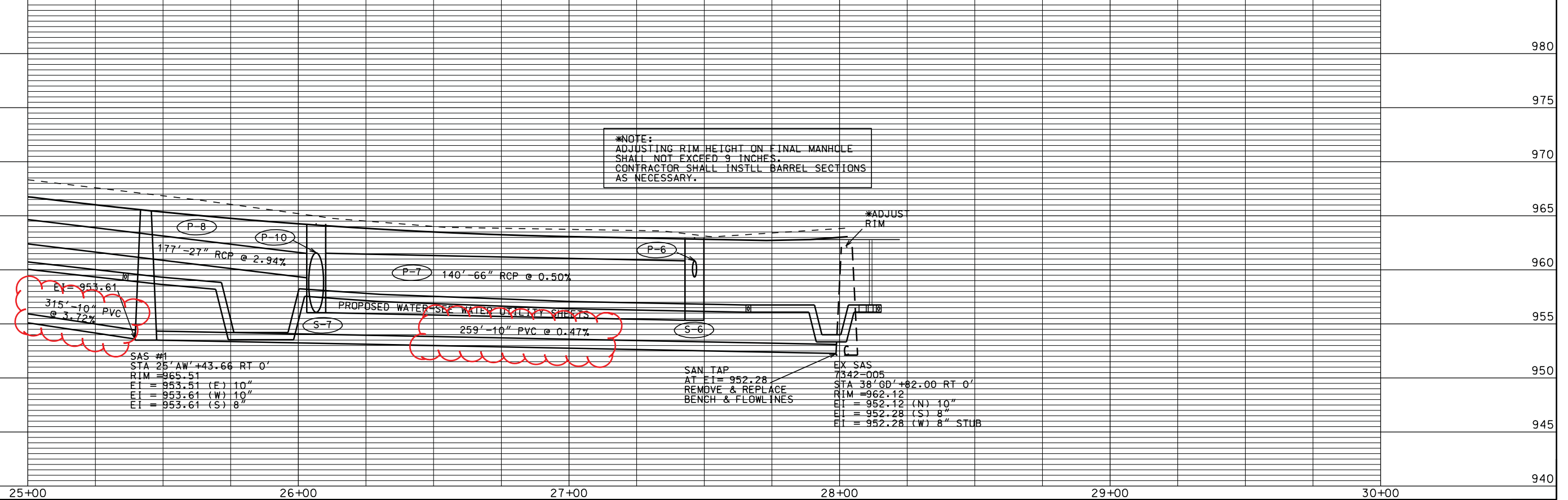
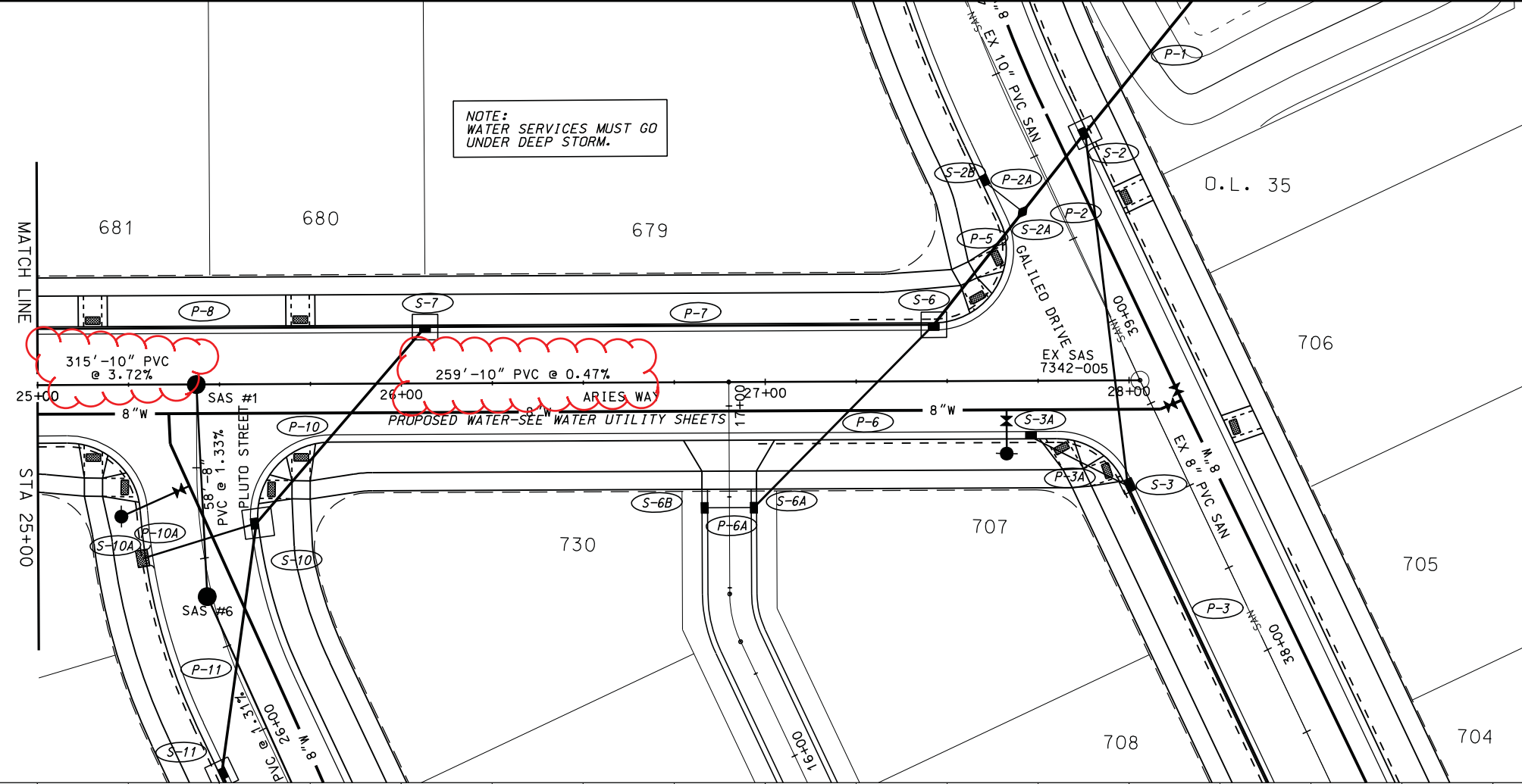


ORIGINATOR: CITY OF MADISON, STREETS DIVISION

UTILITIES
PLAN AND PROFILE
ARIES WAY CITY OF MADISON

REVISED 7/17/14 EEA

NOTE:
WATER SERVICES MUST GO
UNDER DEEP STORM.



P-8
 P-10
 P-7
 S-7
 S-6
 S-10
 S-11
 SAS #1
 STA 25+00 AW +43.66 RT 0'
 RIM = 965.51
 EI = 953.51 (E) 10"
 EI = 953.61 (W) 10"
 EI = 953.61 (S) 8"

SAN TAP
 AT EI = 952.28
 REMOVE & REPLACE
 BENCH & FLOW LINES
 EX SAS
 7342-005
 STA 38+00 RT 0'
 RIM = 962.12
 EI = 952.12 (N) 10"
 EI = 952.28 (S) 8"
 EI = 952.28 (W) 8" STUB

*NOTE:
 ADJUSTING RIM HEIGHT ON FINAL MANHOLE
 SHALL NOT EXCEED 9 INCHES.
 CONTRACTOR SHALL INSTLL BARREL SECTIONS
 AS NECESSARY.

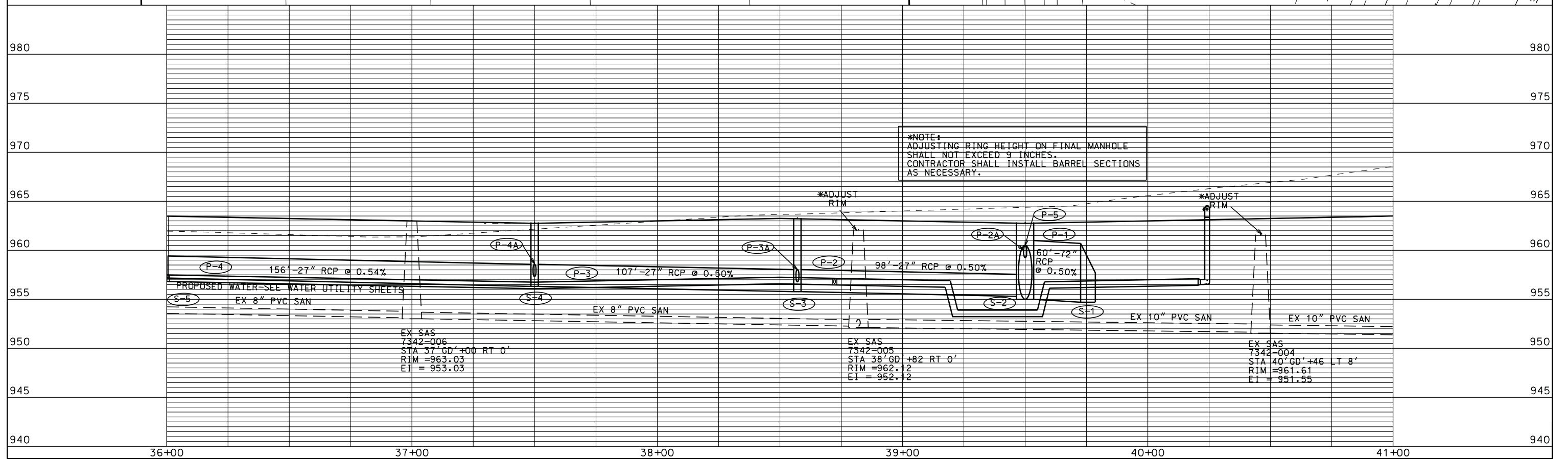
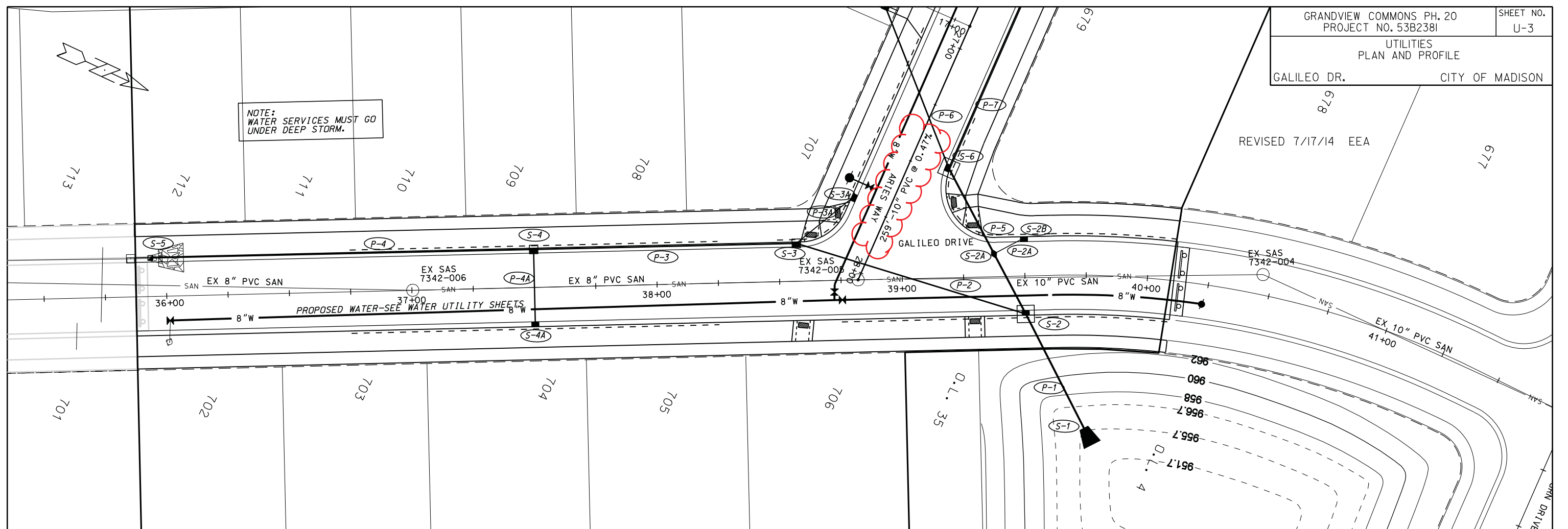
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PLOT NAME:

REV. DATE:

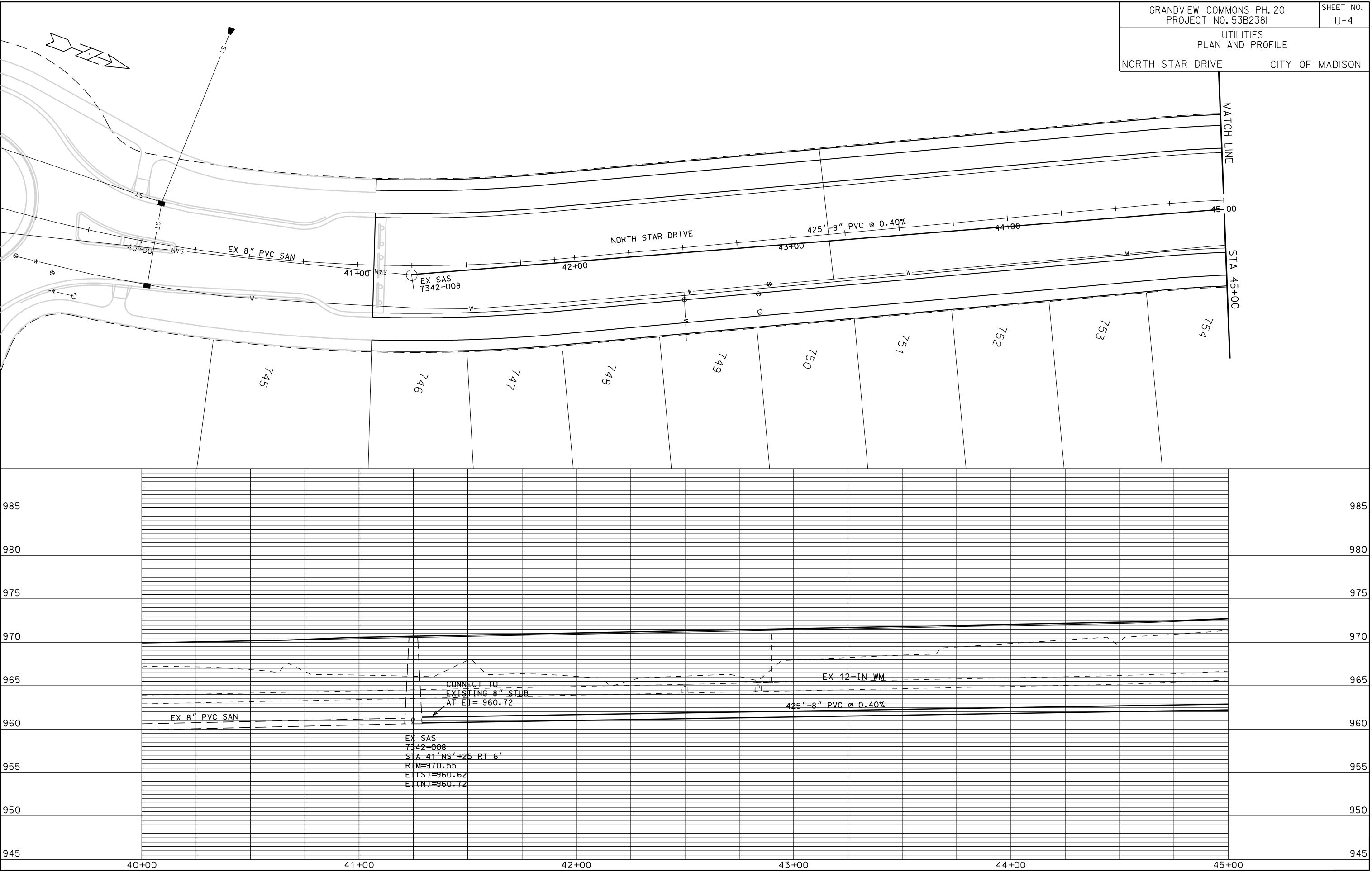
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

NOTE:
WATER SERVICES MUST GO
UNDER DEEP STORM.



ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLOT SCALE: 1"=40'
PLOT NAME: GALILEO DR.
REV. DATE: 7/17/2014



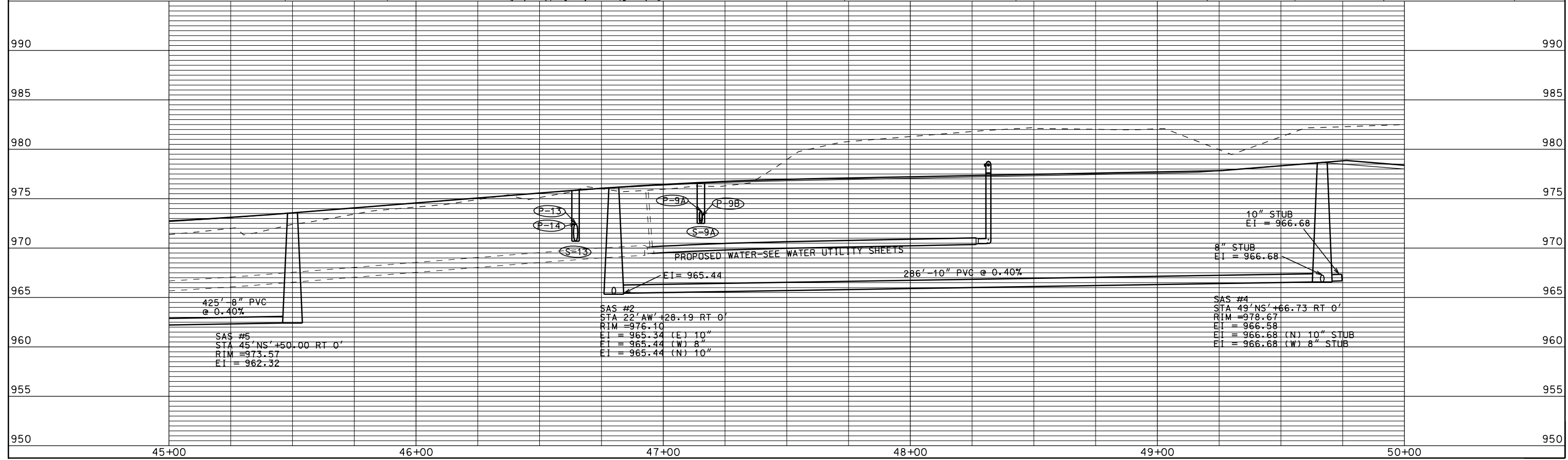
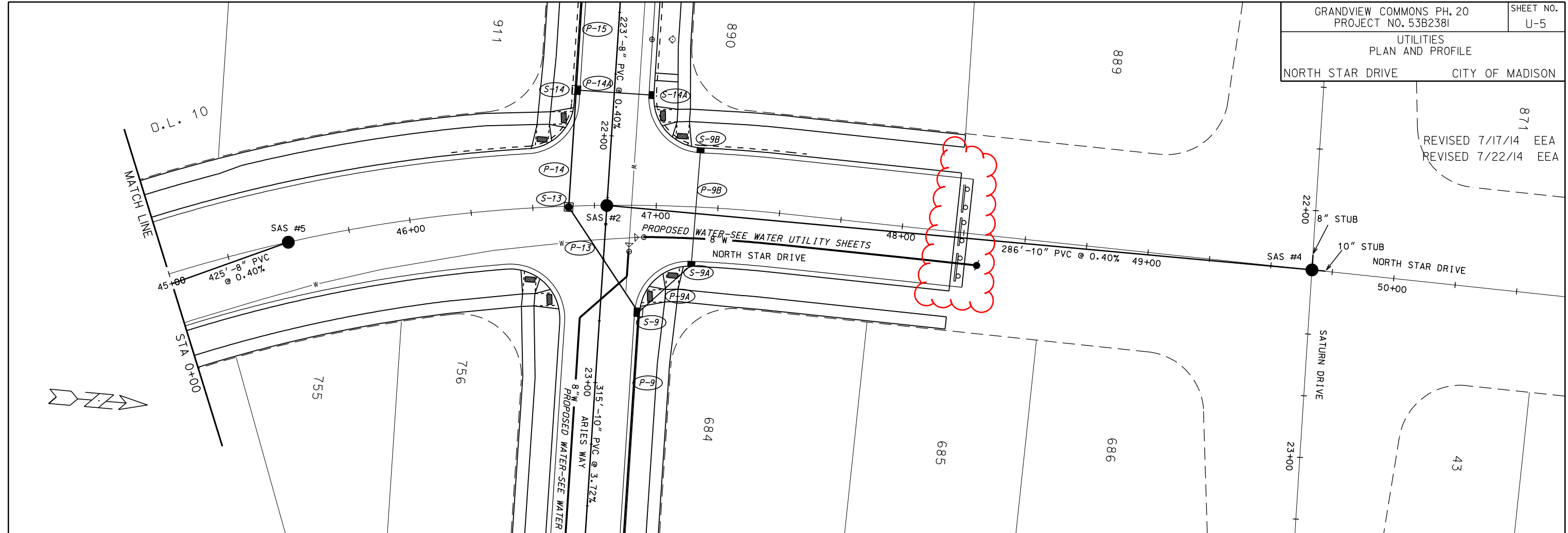
PLOT SCALE:

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REV. DATE:

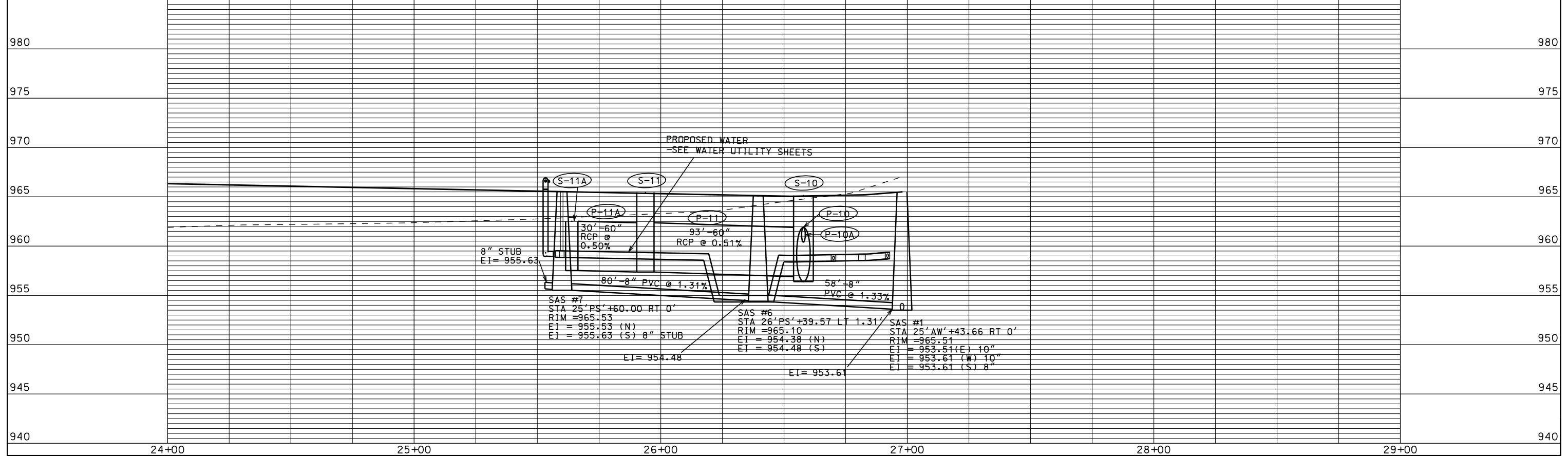
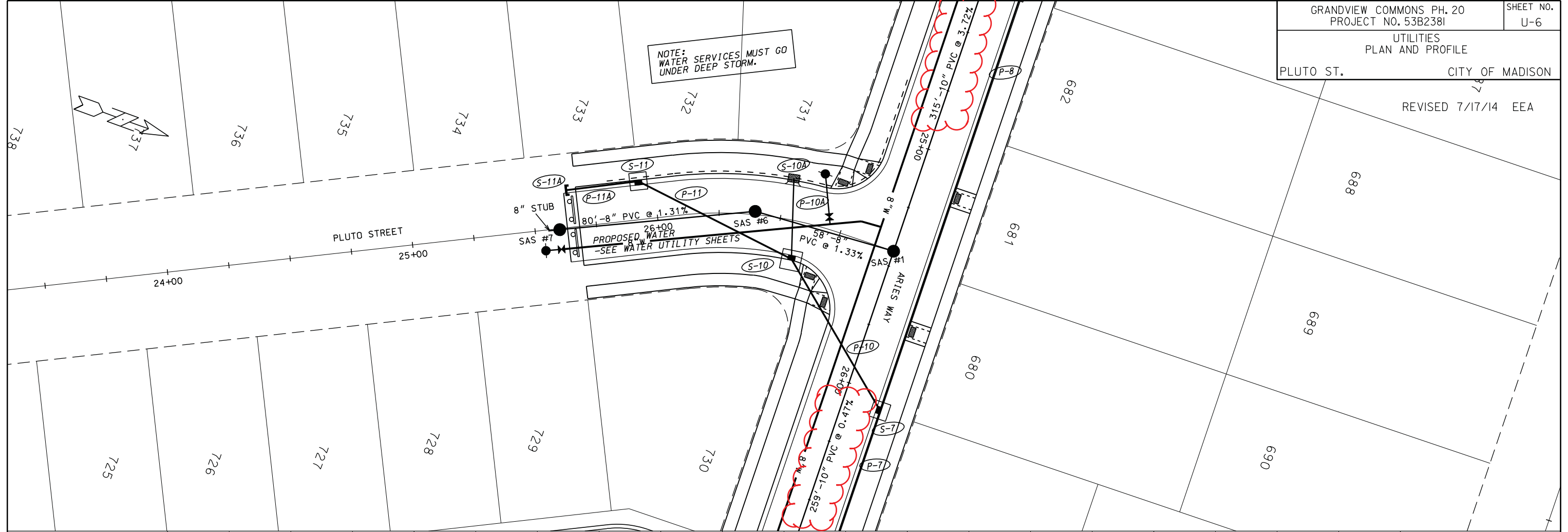
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

871
REVISED 7/17/14 EEA
REVISED 7/22/14 EEA



PLOT SCALE:
PLOT NAME:
REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



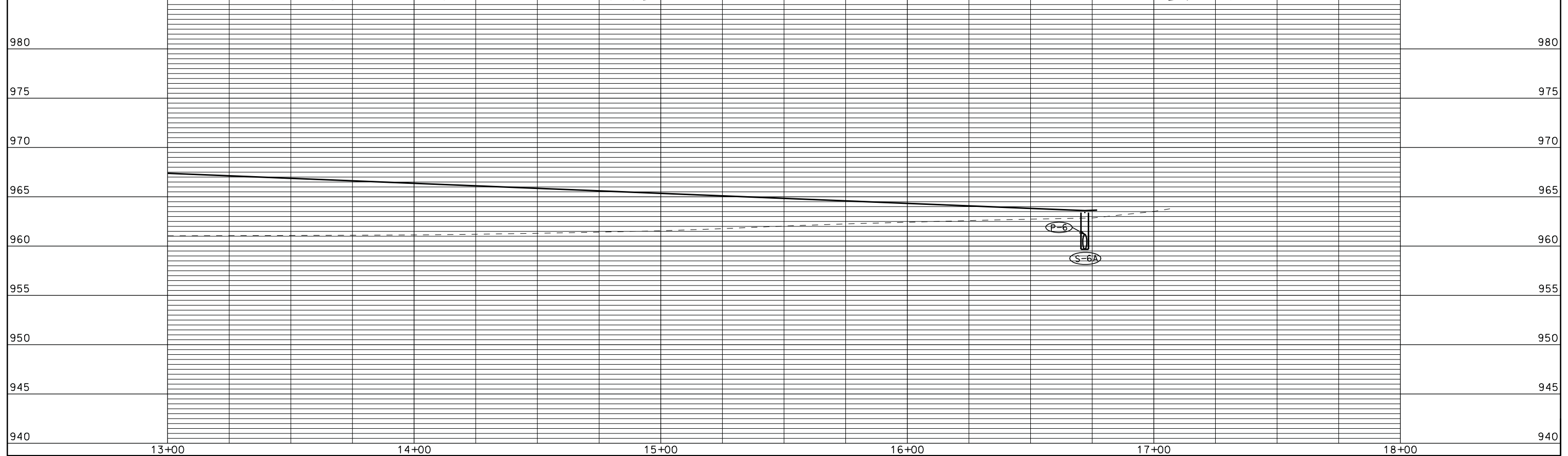
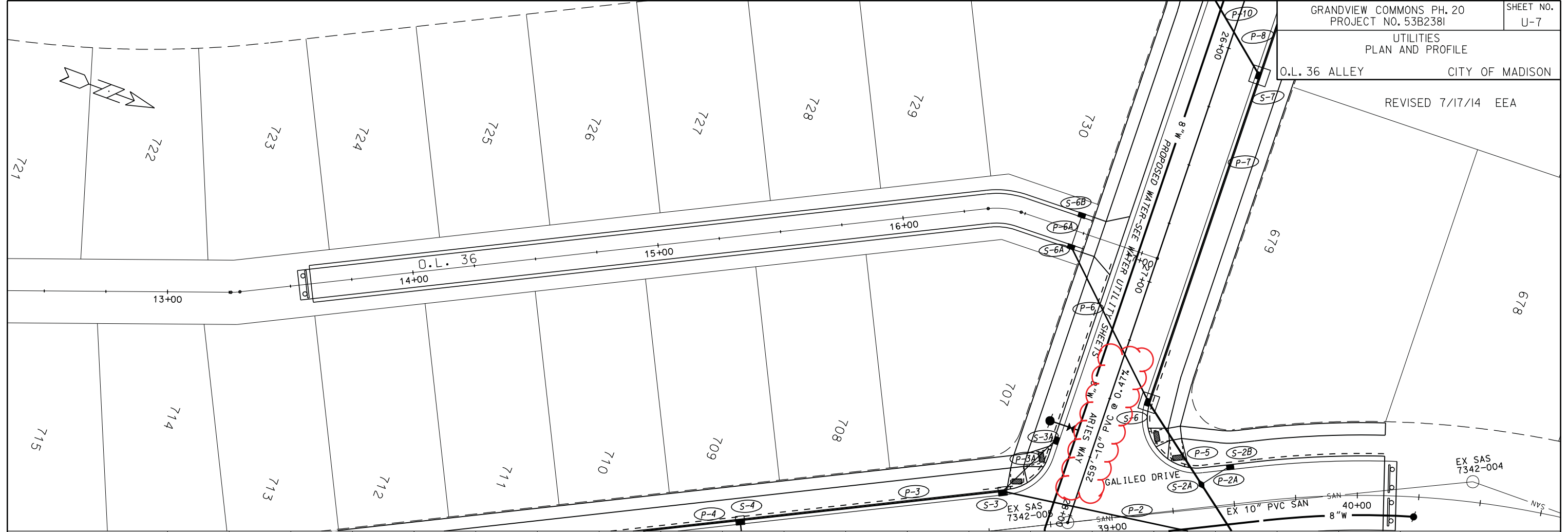
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLOT SCALE: PLOT NAME: REV. DATE:

UTILITIES
PLAN AND PROFILE

O.L. 36 ALLEY CITY OF MADISON

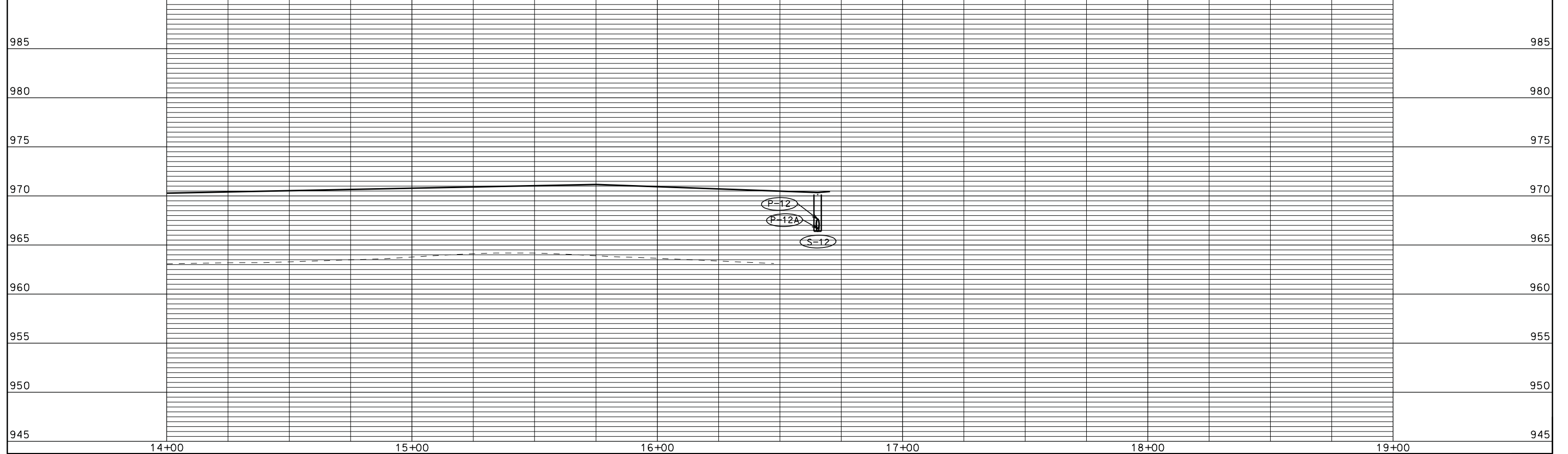
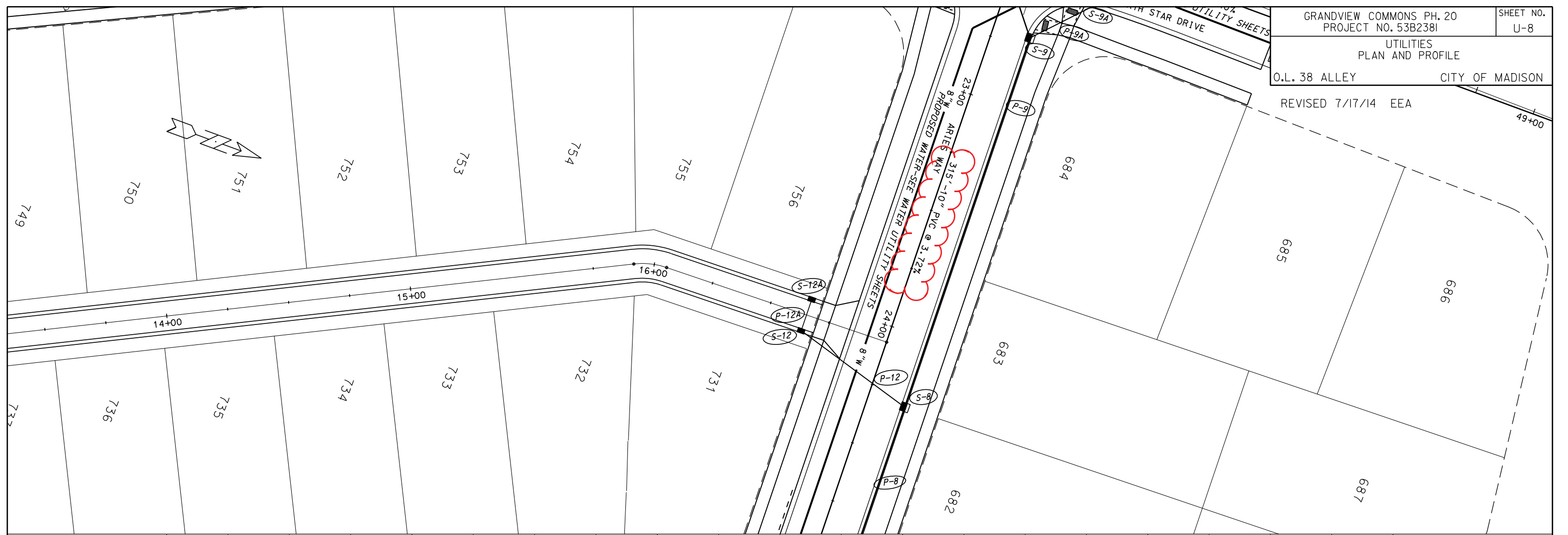
REVISED 7/17/14 EEA



PLOT SCALE:
PLOT NAME:
REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

REVISED 7/17/14 EEA



PLOT SCALE:
 PLOT NAME:
 REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

SANITARY SCHEDULE

ALIGNMENT CODES:

"AW"= ARIES WAY
 "NS"=NORTH STAR DRIVE
 "PS"=PLUTO STREET
 "GD"=GALILEO DRIVE

* REVISED 7/17/14 EEA

GRANDVIEW COMMONS PH. 20
 PROJECT NO. 53B2381

SHEET NO.
 U-9

SANITARY SEWER SCHEDULE

CITY OF MADISON

PROPOSED SANITARY STRUCTURES

SAS NO.	STATION	LOCATION (OFFSET)	TOP OF CASTING	E.I.	DEPTH	NOTES
SAS#1	25'AW'+43.66	CL	965.51	953.51	12.00	-
SAS#2	22'AW'+28.19	CL	976.10	965.34	10.76	-
SAS#3	20'AW'+05.00	CL	977.32	966.33	10.99	-
SAS#4	49'NS'+66.73	CL	978.67	966.58	12.09	-
SAS#5	45'NS'+50.00	CL	973.57	962.42	11.15	-
SAS#6	26'PS'+39.57	LT-1.31	965.10	954.38	10.72	-
SAS#7	25'PS'+60.00	CL	965.53	955.53	10.00	-

PROPOSED SANITARY PIPES

FROM SAS (DWNSTRM)	TO SAS (UPSTREAM)	EI # (DWNSTRM)	EI # (UPSTRM)	LENGTH (FT)	SLOPE (%)	SIZE (DIA)	PVC TYPE	NOTES
* EX SAS 7342-005	SAS#1	952.28	953.51	259	0.47%	10"	SDR-35	-
* SAS#1	SAS#2	953.61	965.34	315	3.72%	10"	SDR-35	-
SAS#2	SAS#3	965.44	966.33	223	0.40%	8"	SDR-35	-
* SAS#2	SAS#4	965.44	966.58	286	0.40%	10"	SDR-35	-
EX SAS 7342-008	SAS#5	960.72	962.42	425	0.40%	8"	SDR-35	-
SAS#1	SAS#6	953.61	954.38	58	1.33%	8"	SDR-35	-
SAS#6	SAS#7	954.48	955.53	80	1.31%	8"	SDR-35	-

ADJUST SANITARY STRUCTURES

STRUCTURE	STATION	LOCATION (OFFSET)	EX. RIM	PROP. RIM	NOTES
SAS 7342-005	38'GD'+82.00	CL	962.12	963.09	ADJUSTING RINGS SHALL NOT EXCEED 9 INCHES
SAS 7342-004	40'GD'+46.00	LT-8.00	961.61	963.23	ADJUSTING RINGS SHALL NOT EXCEED 9 INCHES

REV. DATE: _____ PLOT NAME: _____ PLOT SCALE: _____

STORM SEWER SCHEDULE

ALIGNMENT CODES:

"GD" = GALILEO DR
 "AW" = ARIES WAY
 "OS" = OUTLOT 36
 "NS" = NORTH STAR DRIVE
 "PS" = PLUTO ST
 "OE" = OUTLOT 38

* REVISED 7/17/14 EEA
 + REVISED 7/22/14 EEA

GRANDVIEW COMMONS, PH. 20
 PROJECT NO. 53B2381

SHEET NO.
 U-10

STORM SEWER SCHEDULE CITY OF MADISON

STRUCTURES

STRUC. NO.	STATION	LOCATION (OFFSET)	TYPE	TOP OF CASTING	E.I.	DEPTH	NOTES
S-1	39'GD'+75.65	RT-63.44	72" RCP AE	-	954.70	-	WITH GATE
S-2	39'GD'+50.00	RT-15.50	7X7 SAS	962.85	955.00	7.85	FP; LP; W/ R-3067-7004-VB
S-2A	39'GD'+37.63	LT-9.34	TAP	-	957.26	-	-
S-2B	39'GD'+50.00	LT-15.50	H INLET	962.69	958.38	4.31	LP; W/ R-3067-7004-VB
S-3	38'GD'+57.15	LT-15.50	3X3 SAS	963.30	955.79	7.51	FP; W/ R-1550-0054
S-3A	27'AW'+73.00	RT-15.50	H INLET	962.82	959.22	3.60	LP; W/ R-3067-7004-VB
S-4	37'GD'+50.00	LT-15.50	3X3 SAS	962.85	956.33	6.52	LP; W/ R-3067-7004-VB
S-4A	37'GD'+50.00	H INLET	H INLET	962.75	959.15	3.60	LP; W/ R-3067-7004-VB
S-5	35'GD'+98.49	LT-15.50	CONCRETE COLLAR	-	957.18	-	-
S-6	27'AW'+46.42	LT-15.50	7X7 SAS	962.95	955.34	7.61	FP; W/ R-3067-7004-V
S-6A	16'OS'+72.00	RT-6.75	H INLET	963.41	959.69	3.72	LP; W/ R-3362-L; SEE SDD 5.7.33
S-6B	16'OS'+72.00	LT-6.75	H INLET	963.55	960.01	3.54	LP; W/ R-3362-L; SEE SDD 5.7.33
S-7	26'AW'+06.58	LT-15.50	7X7 SAS	964.25	956.04	8.21	FP; W/ R-3067-7004-V
S-8	24'AW'+29.45	LT-15.50	3X3 SAS	969.43	964.50	4.93	FP; W/ R-3067-7004-V
S-9	22'AW'+70.52	LT-15.50	3X3 SAS	975.45	970.45	5.00	W/ R-3067-7004-V
S-9A	47'NS'+15.26	RT-23.50	H INLET	976.58	972.51	4.07	W/ R-3067-7004-V
S-9B	47'NS'+16.86	LT-23.50	H INLET	976.60	973.00	3.60	W/ R-3067-7004-V
S-10	26'PS'+57.92	RT-15.50	8X8 SAS	965.14	956.40	8.74	W/ R-3067-7004-V
S-10A	26'PS'+52.71	LT-15.50	TERRACE INLET TYPE	964.96	960.57	4.39	FP; LP; PER SDD 5.7.12B; SEE NOTE (1)
S-11	25'PS'+93.82	LT-15.50	7X7 SAS	965.46	957.37	8.09	W/ R-3067-7004-V
S-11A	25'PS'+63.94	LT-15.50	PIPE PLUG	-	957.52	-	-
S-12	16'OE'+65.41	RT-6.75	H INLET	970.15	966.40	3.75	LP; W/ R-3362-L; SEE SDD 5.7.33
S-12A	16'OE'+65.41	LT-6.75	H INLET	970.39	966.79	3.60	LP; W/ R-3362-L; SEE SDD 5.7.33
S-13	22'AW'+29.88	RT-15.42	3X3 SAS	975.86	970.71	5.15	W/ R-1550-0054
S-14	21'AW'+82.47	RT-15.50	3X3 SAS	975.57	970.95	4.62	LP; W/ R-3067-7004-VB
S-14A	21'AW'+82.47	LT-15.50	H INLET	975.64	972.04	3.60	LP; W/ R-3067-7004-VB
*+ S-15	21'AW'+14.00	RT-15.69	PIPE PLUG	-	971.29	-	-

PIPES

PIPE NO.	FROM (DNSTM)	TO (UPSTM)	LGTH (FT)	DISCH. E.I.	INLET E.I.	SLOPE (%)	PIPE SIZE	TYPE	NOTES
P-1	S-1	S-2	60	954.70	955.00	0.50%	72"	RCP	-
P-2	S-2	S-3	98	955.30	955.79	0.50%	27"	RCP	NCM
P-2A	S-2A	S-2B	14	957.26	958.38	8.00%	15"	RCP	NCM
P-3	S-3	S-4	107	955.79	956.33	0.50%	27"	RCP	-
P-3A	S-3	S-3A	30	956.79	959.22	8.10%	15"	RCP	-
P-4	S-4	S-5	156	956.33	957.18	0.54%	27"	RCP	-
P-4A	S-4	S-4A	31	957.33	959.15	5.87%	15"	RCP	-
P-5	S-2	S-6	67	955.00	955.34	0.51%	66"	RCP	NCM
P-6	S-6	S-6A	70	959.34	959.69	0.50%	18"	RCP	-
P-6A	S-6A	S-6B	14	959.94	960.01	0.50%	15"	RCP	-
P-7	S-6	S-7	140	955.34	956.04	0.50%	66"	RCP	-
P-8	S-7	S-8	177	959.29	964.50	2.94%	27"	RCP	-
P-9	S-8	S-9	158	965.00	970.45	3.45%	21"	RCP	NCM
P-9A	S-9	S-9A	29	970.95	972.51	5.38%	15"	RCP	-
P-9B	S-9A	S-9B	47	972.76	973.00	0.51%	12"	RCP	-
P-10	S-7	S-10	71	956.04	956.40	0.51%	66"	RCP	-
P-10A	S-10	S-10A	33	960.40	960.57	0.52%	18"	RCP	-
P-11	S-10	S-11	93	956.90	957.37	0.51%	60"	RCP	-
P-11A	S-11	S-11A	30	957.37	957.52	0.50%	60"	RCP	-
P-12	S-8	S-12	52	965.50	966.40	1.73%	15"	RCP	-
P-12A	S-12	S-12A	14	966.65	966.79	1.00%	12"	RCP	-
P-13	S-9	S-13	51	970.45	970.71	0.51%	21"	RCP	-
P-14	S-13	S-14	47	970.71	970.95	0.51%	21"	RCP	-
P-14A	S-14	S-14A	31	971.45	972.04	1.90%	15"	RCP	-
*+ P-15	S-14	S-15	68	970.95	971.29	0.50%	21"	RCP	-

SPECIFIC NOTES

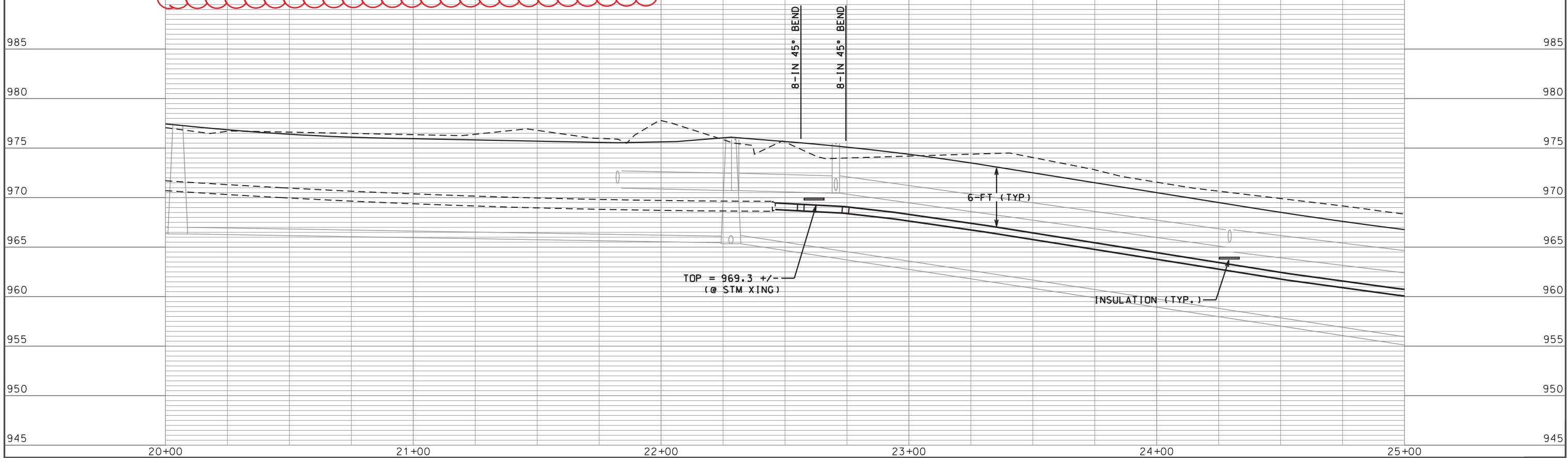
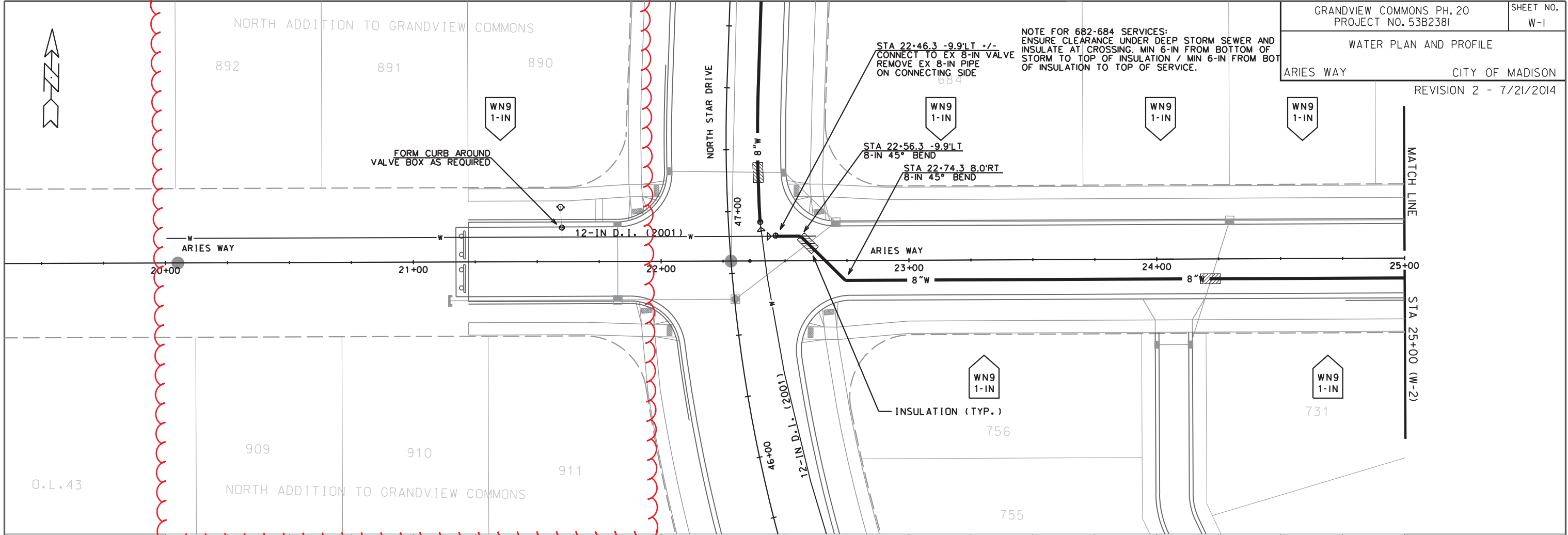
(1) CASTING ELEVATION 0.2' BELOW TOC

STANDARD NOTES:

- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD Poured STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES
- APPROXIMATE DISCHARGE E.I. GIVEN, ADJUST E.I. AND PIPE SLOPE IN THE FIELD.
- TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SAS's.
- TOP OF CONCRETE ROOF (TR) IS 1.25' BELOW TOP OF CASTING UNLESS OTHERWISE NOTED.
- ALL REINFORCED CONCRETE PIPES TO BE CLASS III UNLESS OTHERWISE NOTED.
- SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.
- ALL STRUCTURES CALLED OUT AS FIELD Poured SHALL BE FIELD Poured. ALL OTHER STRUCTURES (NOT INDICATED AS FIELD Poured) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL IF PRECAST STRUCTURES ARE PREFERRED. CONTACT ELIA E ACOSTA OF CITY ENGINEERING AT (608) 266-4096 FOR PRECAST APPROVALS, OR FAX SHOP DRAWINGS TO (608)264-9275.

PLOT SCALE: --- PLOT NAME: --- REV. DATE: ---

ORIGINATOR: CITY OF MADISON STREETS DIVISION



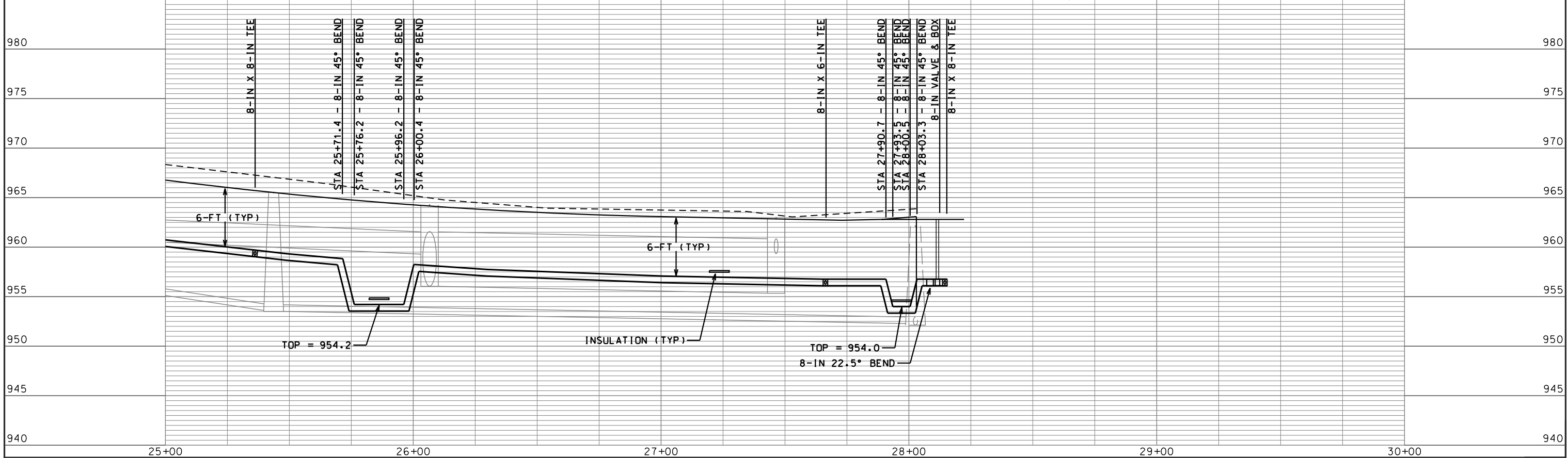
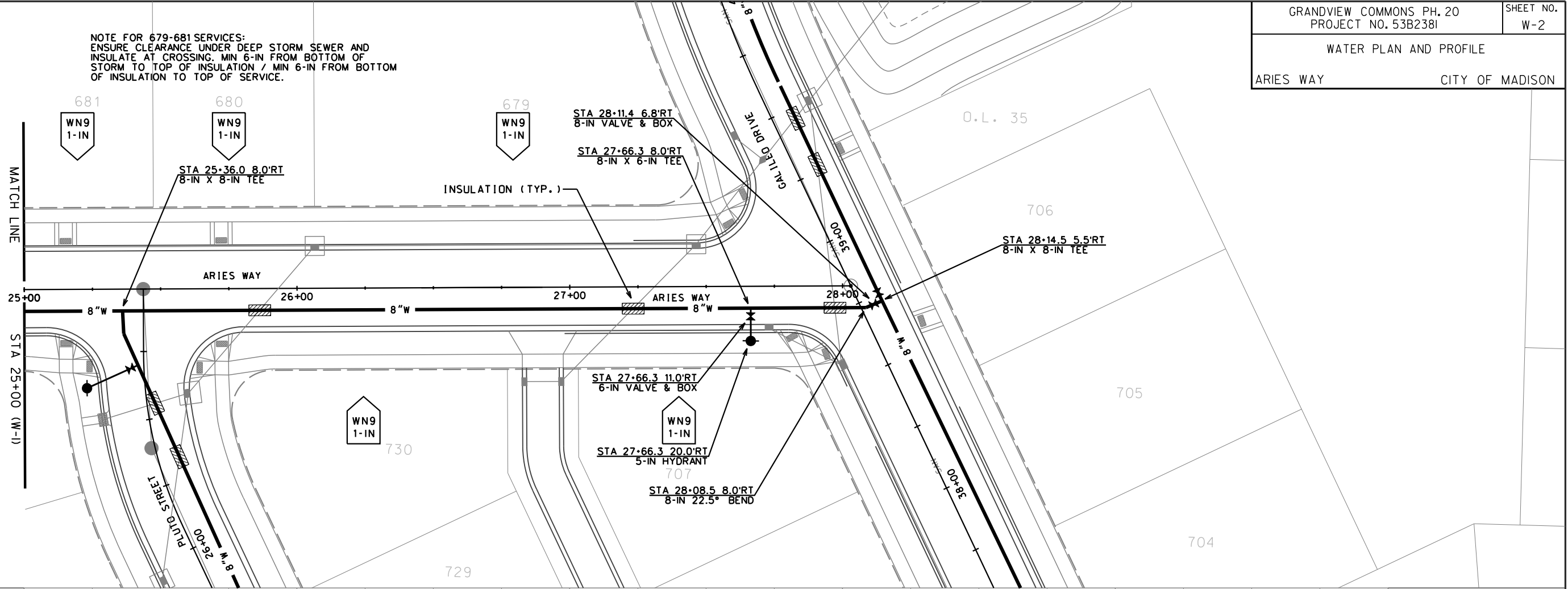
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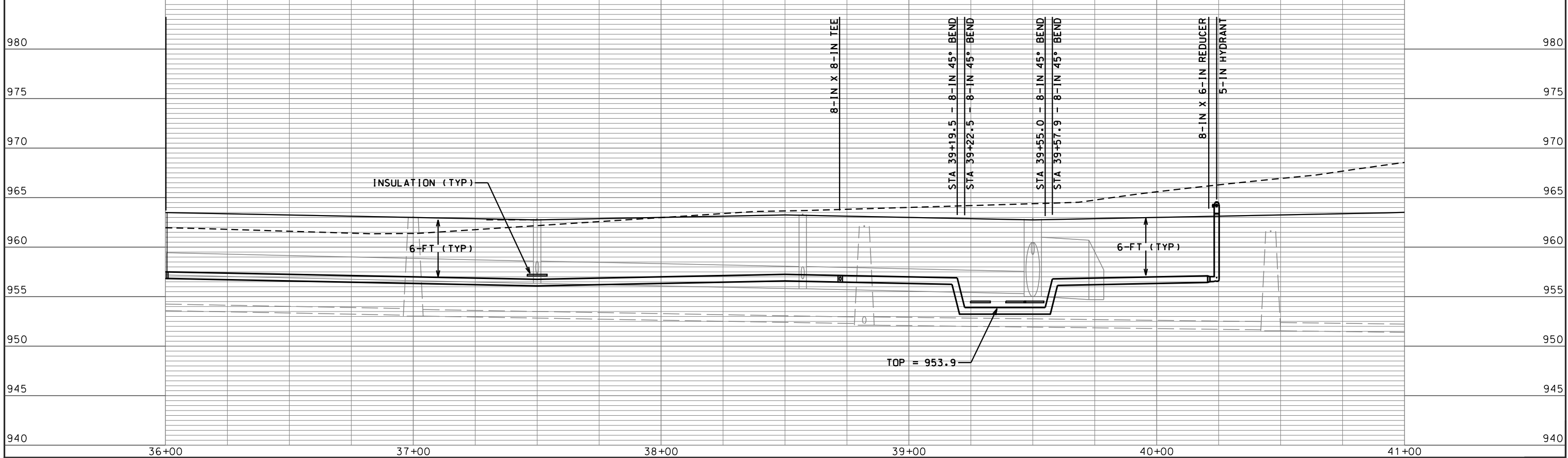
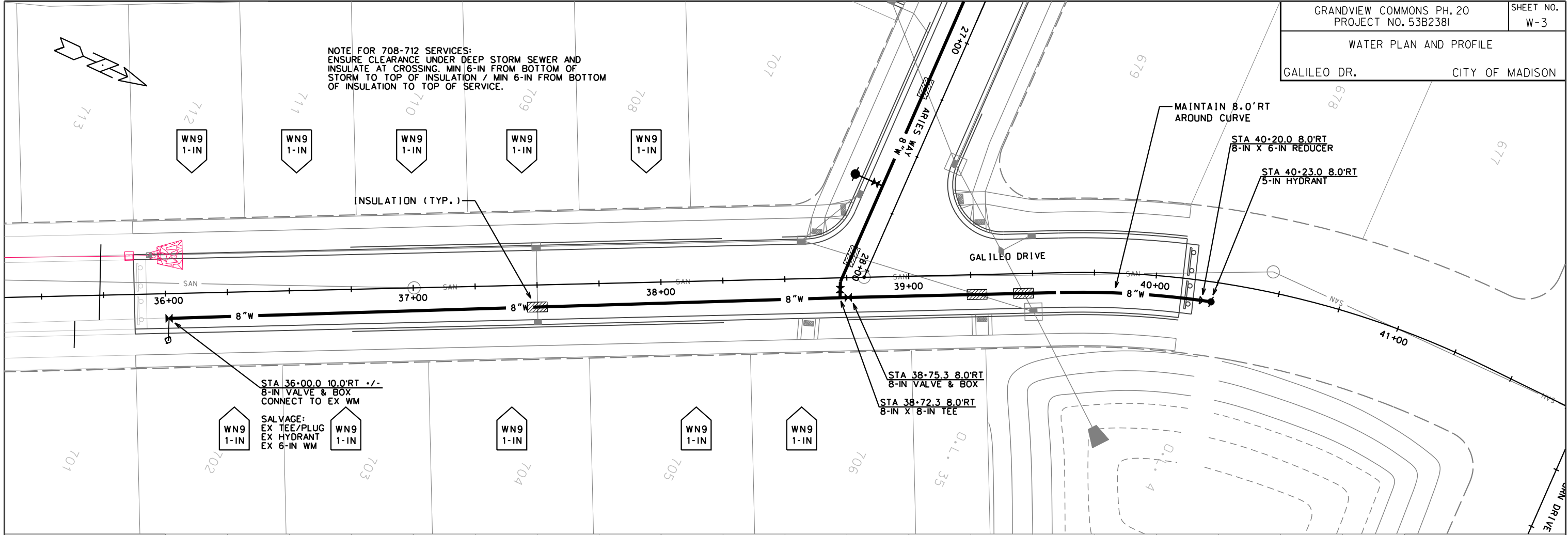
ORIGINATOR: CITY OF MADISON, STREETS DIVISION

NOTE FOR 679-681 SERVICES:
ENSURE CLEARANCE UNDER DEEP STORM SEWER AND
INSULATE AT CROSSING. MIN 6-IN FROM BOTTOM OF
STORM TO TOP OF INSULATION / MIN 6-IN FROM BOTTOM
OF INSULATION TO TOP OF SERVICE.



PLOT SCALE:
 PLOT NAME:
 REV. DATE:
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION

NOTE FOR 708-712 SERVICES:
ENSURE CLEARANCE UNDER DEEP STORM SEWER AND
INSULATE AT CROSSING. MIN 6-IN FROM BOTTOM OF
STORM TO TOP OF INSULATION / MIN 6-IN FROM BOTTOM
OF INSULATION TO TOP OF SERVICE.



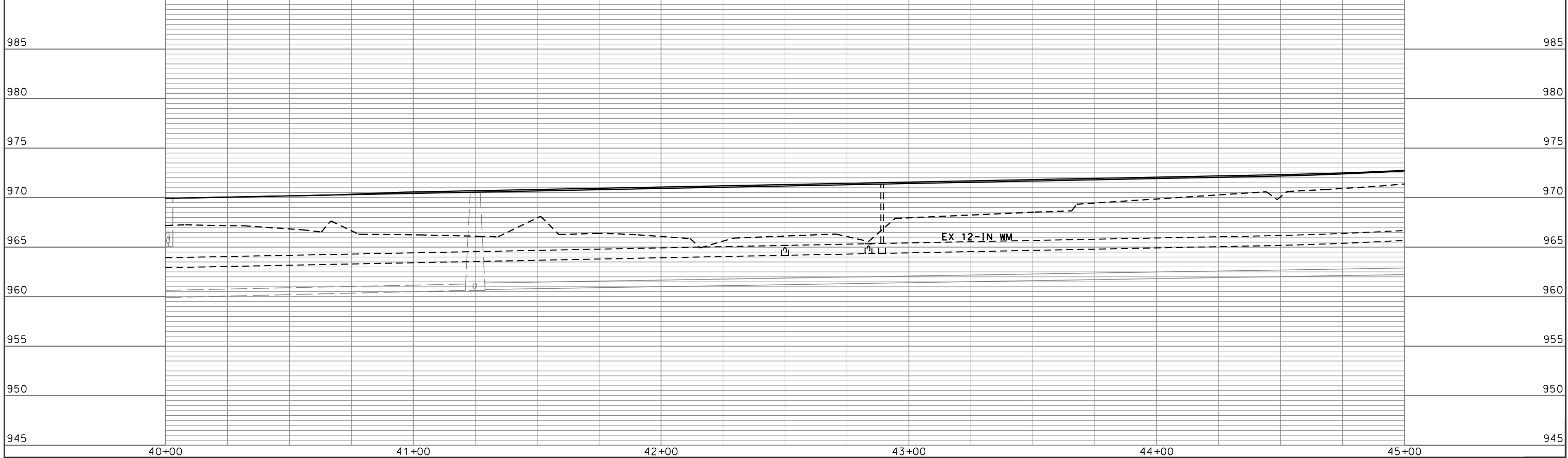
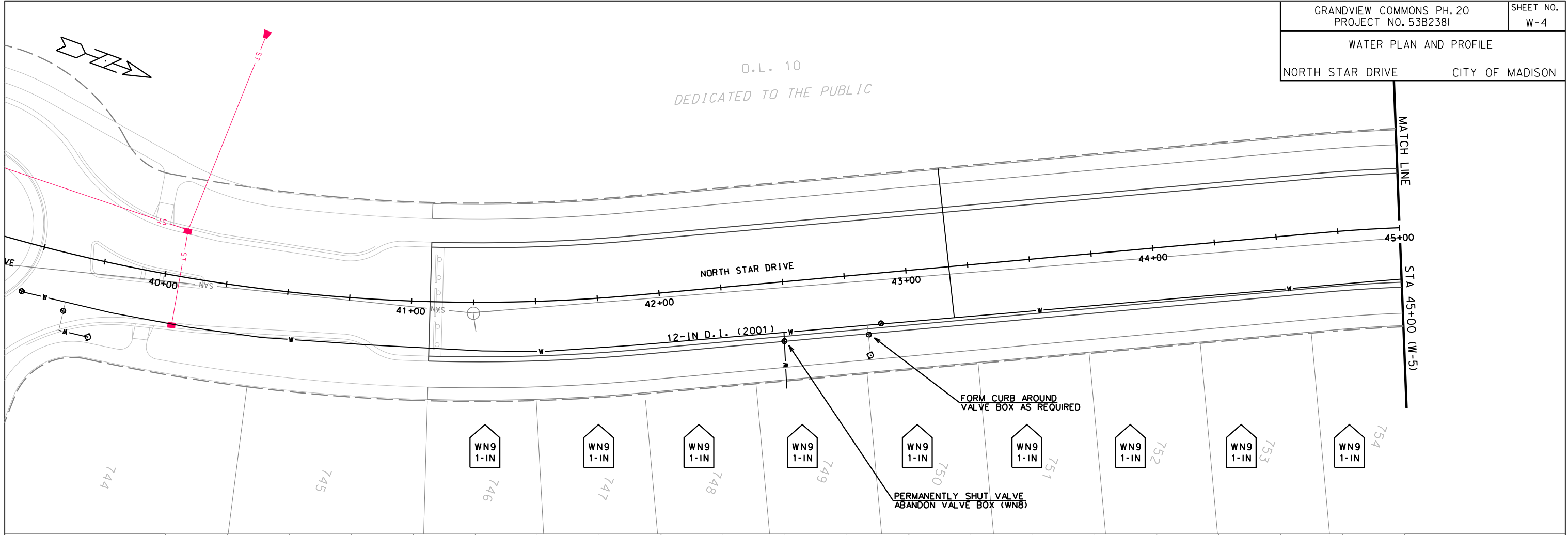
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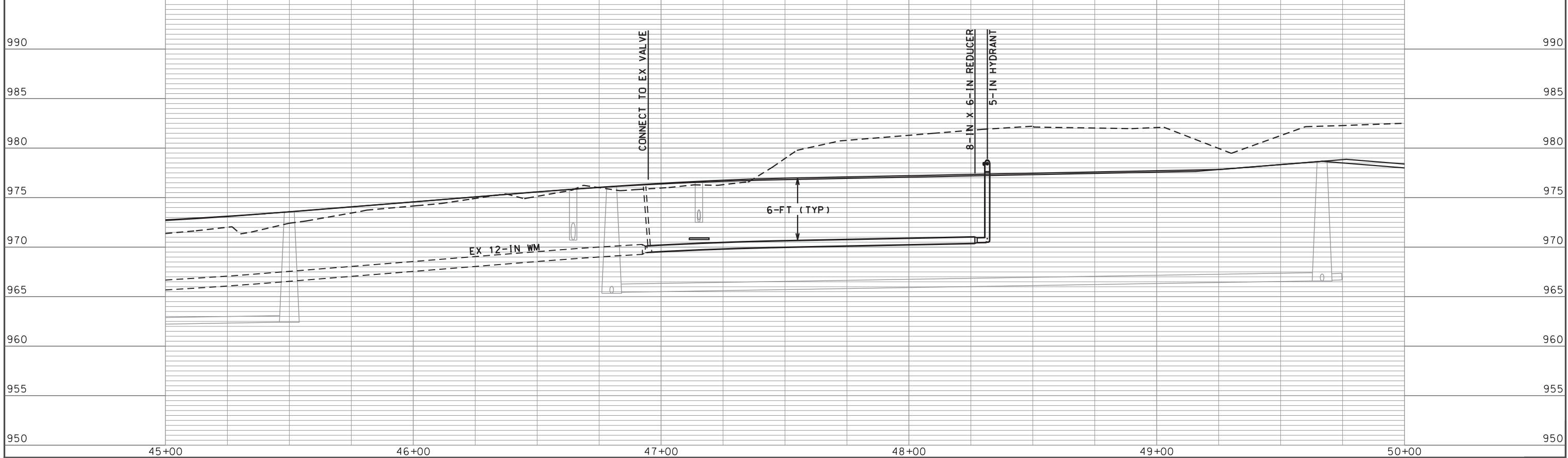
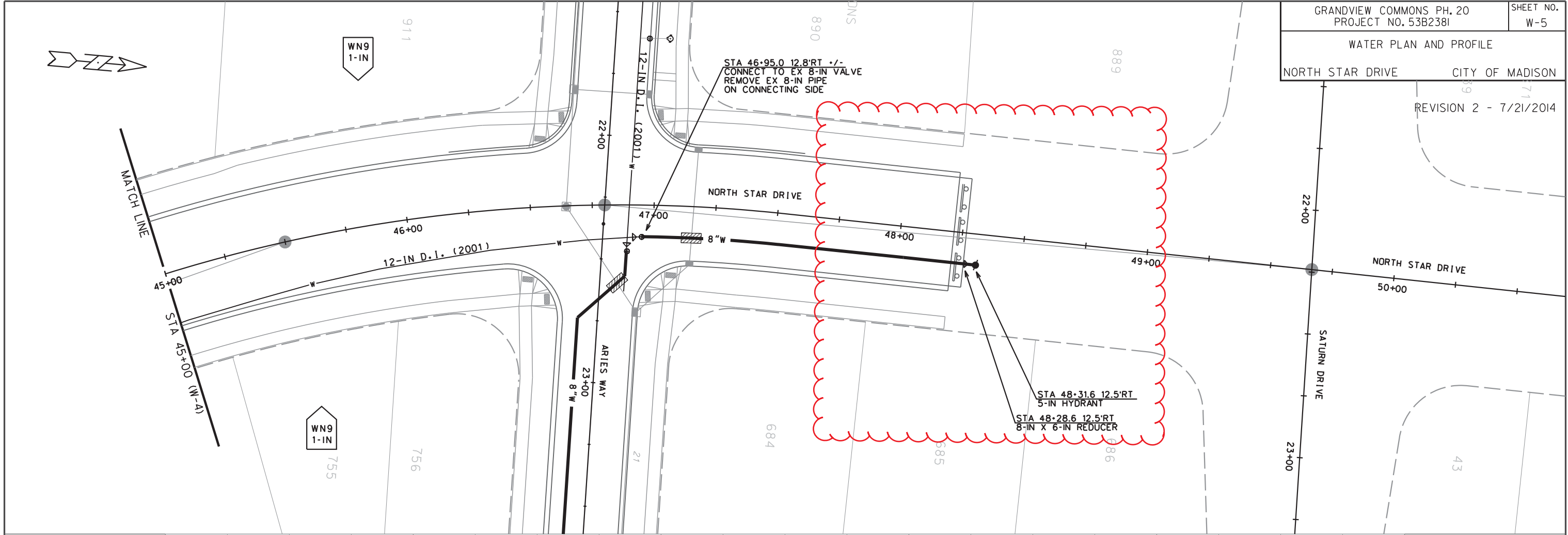
O.L. 10
DEDICATED TO THE PUBLIC



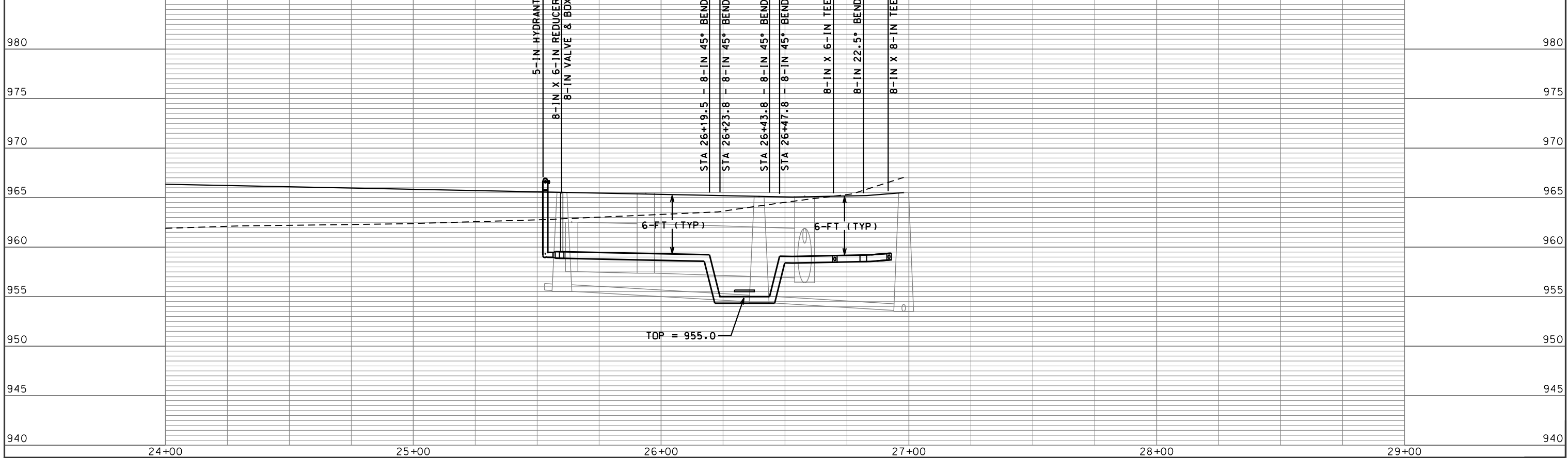
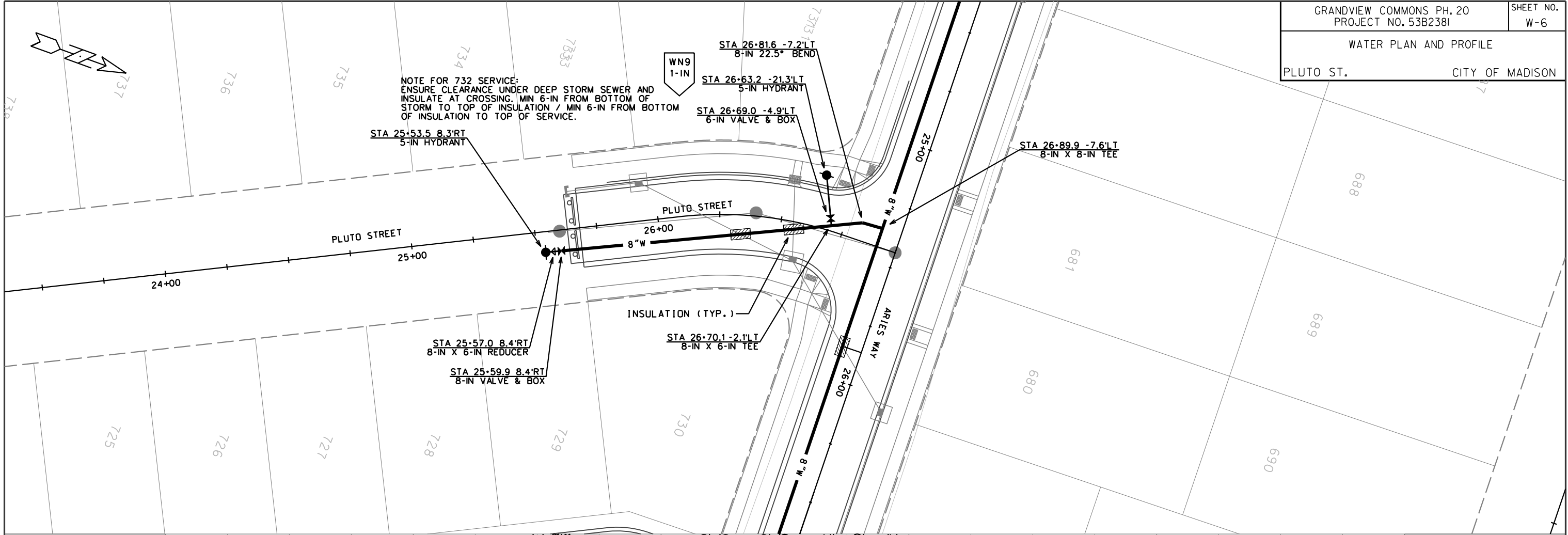
PLOT SCALE:
PLOT NAME:
REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION

REVISION 2 - 7/21/2014



PLOT SCALE:
 PLOT NAME:
 REV. DATE:
 ORIGINATOR: CITY OF MADISON STS DIVISION



PLOT SCALE:
 PLOT NAME:
 REV. DATE:
 ORIGINATOR: CITY OF MADISON, STREETS DIVISION

CONNECT TO EX 8-IN VALVE (V-5)
-TURN VALVES:
*V-5 (SHOULD BE OFF ALREADY)
-NOTIFY
*NONE

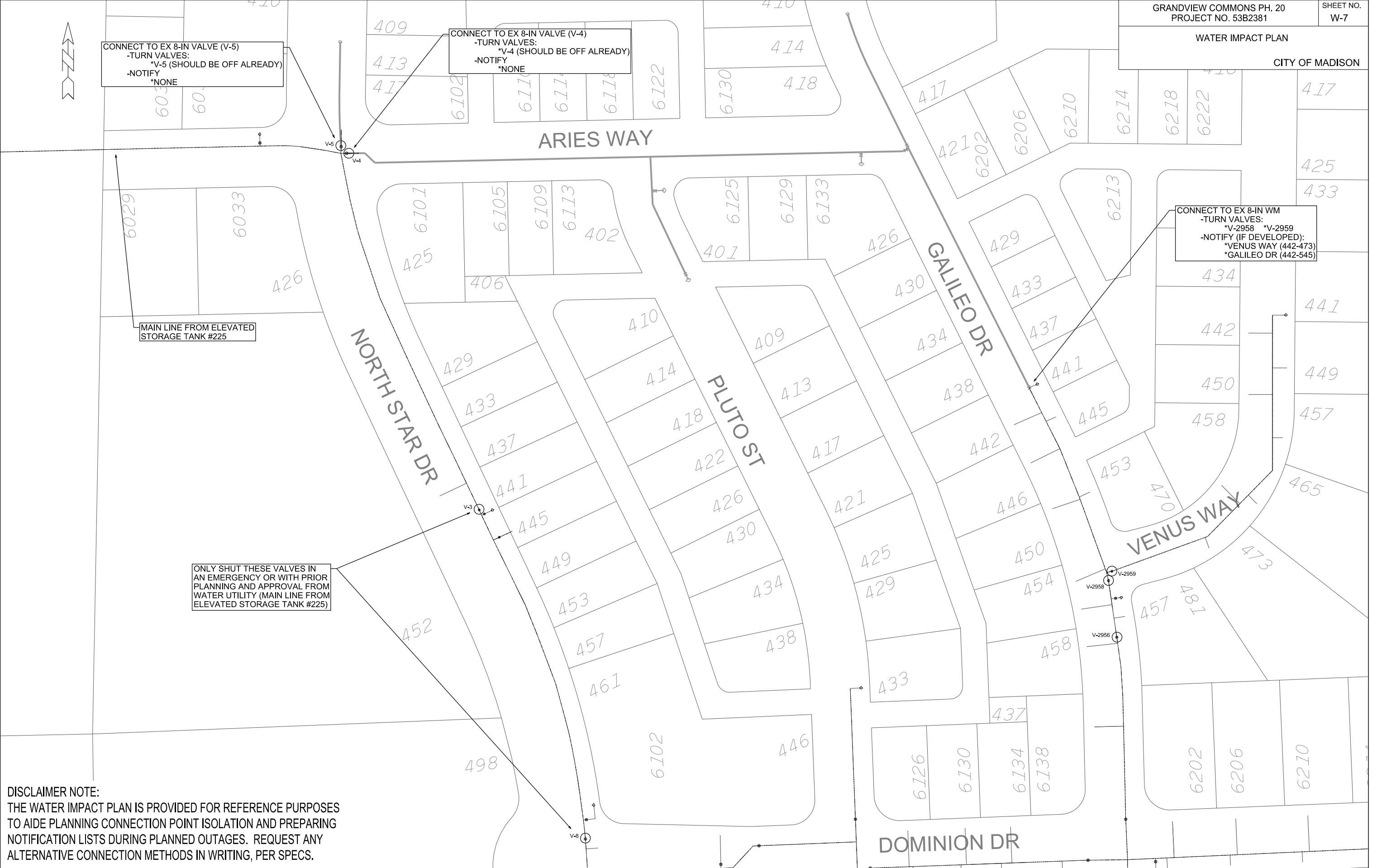
CONNECT TO EX 8-IN VALVE (V-4)
-TURN VALVES:
*V-4 (SHOULD BE OFF ALREADY)
-NOTIFY
*NONE

CONNECT TO EX 8-IN WM
-TURN VALVES:
*V-2958 *V-2959
-NOTIFY (IF DEVELOPED):
*VENUS WAY (442-473)
*GALILEO DR (442-545)

MAIN LINE FROM ELEVATED
STORAGE TANK #225

ONLY SHUT THESE VALVES IN
AN EMERGENCY OR WITH PRIOR
PLANNING AND APPROVAL FROM
WATER UTILITY (MAIN LINE FROM
ELEVATED STORAGE TANK #225)

DISCLAIMER NOTE:
THE WATER IMPACT PLAN IS PROVIDED FOR REFERENCE PURPOSES
TO AIDE PLANNING CONNECTION POINT ISOLATION AND PREPARING
NOTIFICATION LISTS DURING PLANNED OUTAGES. REQUEST ANY
ALTERNATIVE CONNECTION METHODS IN WRITING, PER SPECS.



CONSTRUCTION NOTES:

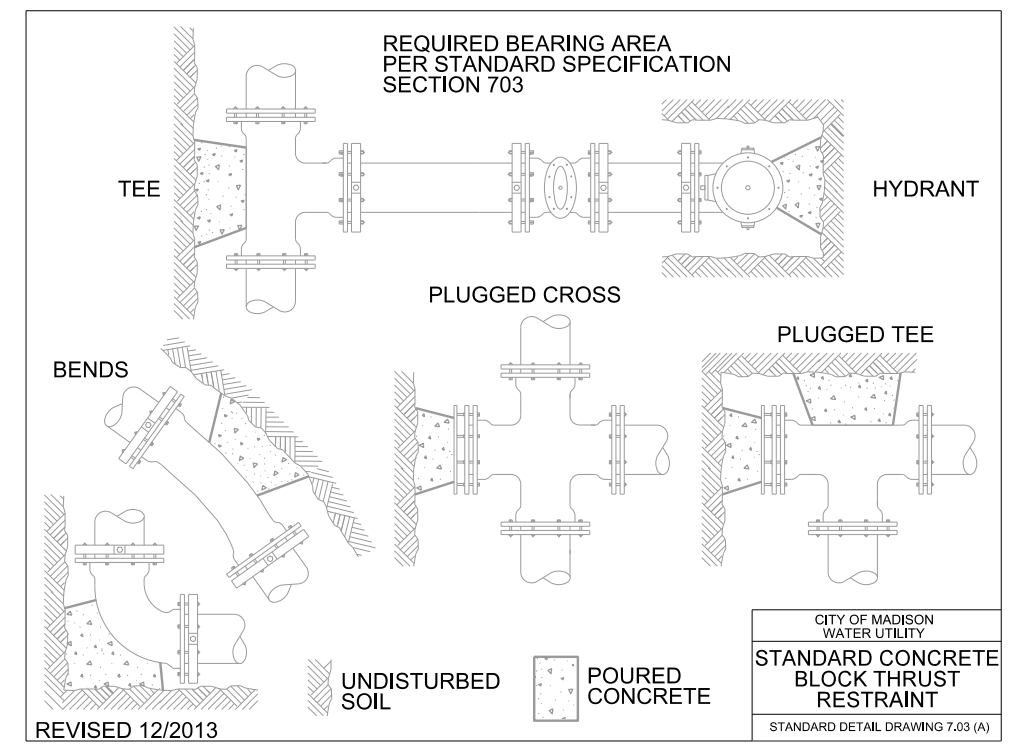
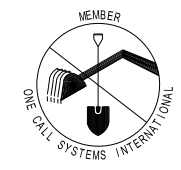
1. CONSTRUCT NEW WATER MAIN 6.0' BELOW FINISHED GRADE, UNLESS OTHERWISE NOTED. INSULATE MAIN WITH POLYSTYRENE BOARD AT UTILITY CROSSINGS OR OTHER AREAS IDENTIFIED BY ENGINEER AS HAVING INADEQUATE COVER.
2. VERIFY SIZE OF EXISTING WATER SERVICES AND RECONNECT SERVICES AS INDICATED.
3. MINIMIZE DISTRUPTION OF SERVICE TO EXISTING CUSTOMERS. NOTIFY PER CONTRACT REQUIREMENTS OF ANY PLANNED WATER OUTAGE.
4. THE EXISTING UTILITIES SHOWN ON THIS PLAN REPRESENT THE BEST INFORMATION AVAILABLE TO THE WATER UTILITY AT THE TIME OF PLAN PREPARATION. CONTRACTOR IS RESPONSIBLE FOR HAVING EACH UTILITY LOCATED PRIOR TO COMMENCING WORK.

- WN1 REPLACE THE EXISTING LEAD SERVICE WITH A NEW COPPER SERVICE.
- WN2 EXTEND AND RECONNECT THE EXISTING COPPER SERVICE TO THE NEW WATER MAIN.
- WN3 EXISTING SERVICE TO BE ABANDONED WHEN THE WATER MAIN IS CUT OFF.
- WN4 DISCONNECT FROM THE OLD WATER MAIN AND RECONNECT THE EXISTING COPPER WATER SERVICE LATERAL TO THE NEW WATER MAIN.
- WN5 RELOCATE THE EXISTING FIRE HYDRANT.
- WN6 ABANDON WATER VALVE ACCESS STRUCTURE.
- WN7 FURNISH AND INSTALL THE NEW TOP SECTION FOR THE WATER ACCESS STRUCTURE.
- WN8 ABANDON THE VALVE BOX.
- WN9 FURNISH THE DITCH, COMPACTION, AND ALL MATERIALS AND LABOR FOR THE INSTALLATION OF NEW SERVICE LATERAL.
- WN10 REMOVE AND SALVAGE EXISTING HYDRANT
- WN11 REPLACE THE EXISTING COPPER SERVICE WITH A COPPER SERVICE

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

CALL DIGGERS HOTLINE
TOLL FREE
811 OR 1-800-242-8511
FAX-A-LOCATE 1-800-338-3860
TDD (FOR HEARING IMPAIRED) 1-800-542-2289

WIS. STATUTE 182.0175 (1974)
REQUIRES MIN. OF 3 WORK DAYS
NOTICE BEFORE YOU EXCAVATE.

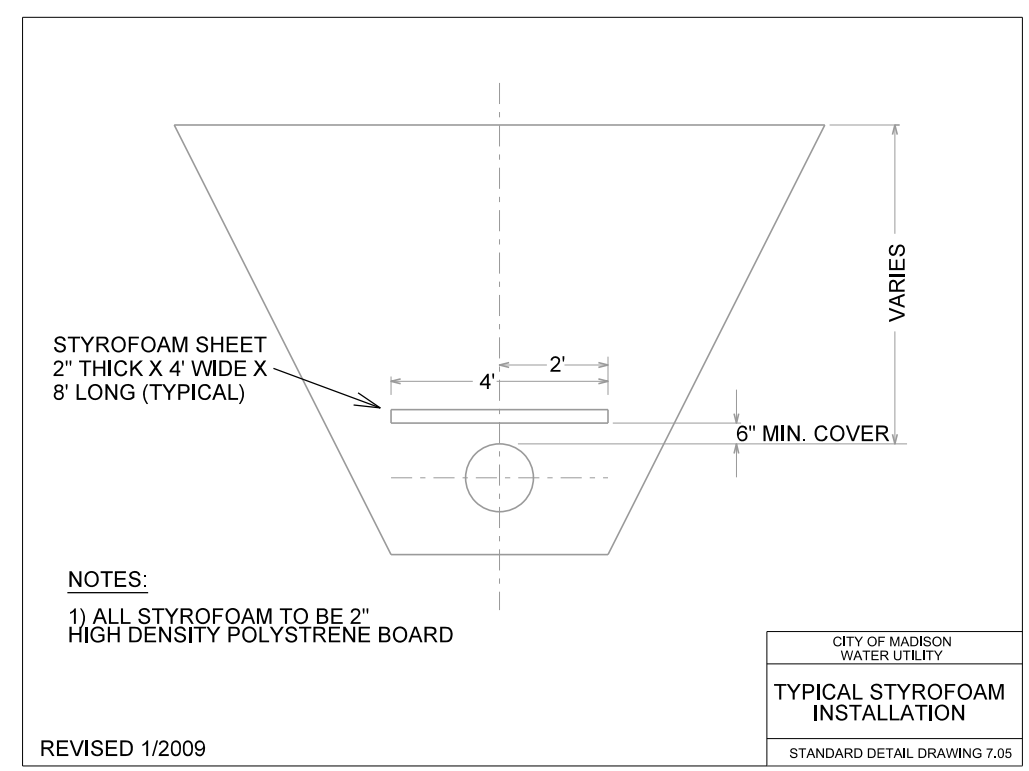
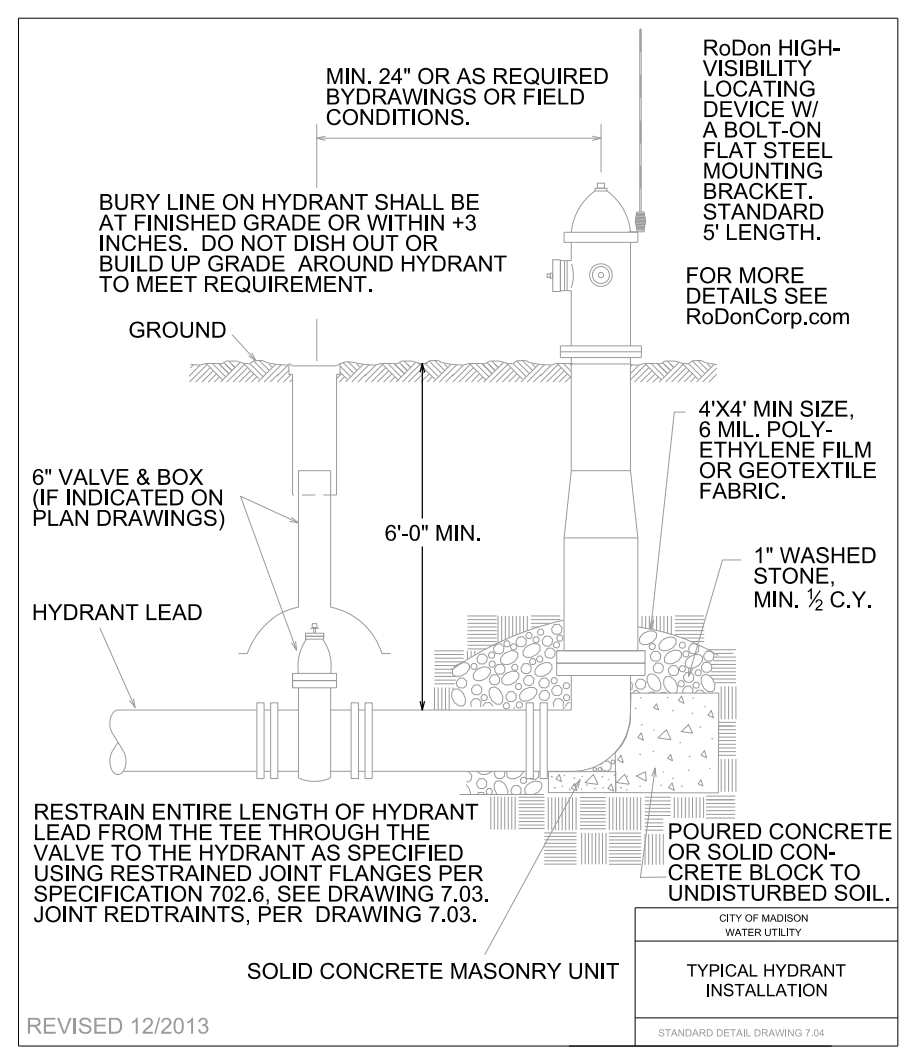


MATERIALS SUPPLIED BY CONTRACTOR*:

- 40-FT - 6-IN PIPE
- 1,240-FT - 8-IN PIPE
- 1,410-FT - POLYWRAP
- 2 - 6-IN VALVE & BOX
- 4 - 8-IN VALVE & BOX
- 2 - 8-IN X 6-IN TEE
- 2 - 8-IN X 8-IN TEE
- 2 - 8-IN 22.5° BEND
- 22 - 8-IN 45° BEND
- 3 - 8-IN X 6-IN REDUCER
- 5 - 5-IN HYDRANT
- 160-FT - 2-IN STYROFOAM INSULATION

SALVAGED MATERIALS*:

- 1 - 5-IN HYDRANT
- 1 - 8-IN X 6-IN TEE



* ESTIMATES FOR INFORMATION ONLY. ENGINEER DOES NOT GUARANTEE ACCURACY OF MATERIAL TAKE-OFFS.

PLOT SCALE: _____ PLOT NAME: _____ REV. DATE: _____ ORIGINATOR: CITY OF MADISON, STREETS DIVISION